



LAKEVIEW VILLAGE

DEVELOPMENT MASTER PLAN



DRAFT - OCTOBER 2018



TABLE OF CONTENTS

Executive Summary	iii	The Plan	37
Introduction	1	5.1 VISION	37
1.1 LAKEVIEW COMMUNITY PARTNERS LIMITED (LCPL)	3	5.2 DISTRICTS & NEIGHBOURHOODS	39
1.2 THE CONSULTANT TEAM	3	5.2.1 LAKEVIEW SQUARE	41
Background	7	5.2.2 WATERWAY COMMON	43
2.1 HISTORIC OVERVIEW	8	5.2.3 INSPIRATION POINT	45
2.2 INSPIRATION LAKEVIEW & THE COMMUNITY PLANNING PROCESS	10	5.2.4 SERSON INNOVATION CORRIDOR	46
2.2.1 IMPLEMENTING INSPIRATION LAKEVIEW	11	5.2.5 THE MARINA	47
2.2.2 THE JIM TOVEY LEGACY	11	5.2.6 OGDEN GREEN	48
2.2.3 TIMELINE OF PROJECT & MASTER PLANNING PROCESS	12	5.2.7 LAKESHORE GATEWAY	49
2.3 LAKEVIEW VILLAGE COMMUNITY RE-ENGAGEMENT	14	5.2.8 FUTURE DISTRICTS	50
2.3.1 COMMUNITY RE-ENGAGEMENT EVENT - APRIL 18, 2018	15	5.3 WATERFRONT, PARKS & OPEN SPACES	51
2.4 POLICY CONTEXT	16	5.3.1 OPEN SPACE CONNECTIVITY	51
2.5 KEY UPDATES TO THE PLAN	17	5.3.2 SERSON CREEK	54
Site Context	21	5.3.3 PARK TYPOLOGIES & PROGRAMMING	55
Structuring Elements	27	5.3.4 SUPPORTIVE OPEN SPACE - PRIVATE	75
4.1 THE '6 BIG MOVES'	27	5.4 ACTIVE TRANSPORTATION	76
4.2 A CONTINUOUS WATERFRONT	28	5.4.1 PEDESTRIAN REALM	77
4.3 A BLUE & GREEN NETWORK	29	5.5 RETAIL, CULTURE & EMPLOYMENT USES	78
4.4 A FINE GRAIN STREET PATTERN	30	5.5.1 RETAIL	78
4.5 BRINGING TRANSIT TO THE SITE	31	5.5.2 CULTURE	79
4.6 A CULTURAL HUB AT THE HEAD OF THE PIERS	32	5.5.3 EMPLOYMENT USES	80
4.7 EMPLOYMENT & INNOVATION CORRIDOR	33	5.5.4 ACTIVATION	81
4.8 PLAN COMPARISON	34	5.6 DEVELOPMENT PROGRAM	82
		5.6.1 UNIT & BUILT FORM COUNTS	83
		5.6.2 HEIGHT & DENSITY DISTRIBUTION	83

5.7	BUILT FORM	84
5.7.1	RESIDENTIAL BUILDING TYPOLOGIES AND BLOCKS	92
5.7.2	VIEWSHEDS, HEIGHTS, & DENSITY	97
5.7.3	RETAIL FACILITIES & AMENITIES	99
5.7.4	CULTURAL FACILITIES & AMENITIES	100
5.7.5	PARKING & SERVICING STRATEGY	101
5.8	SUSTAINABILITY STRATEGY OVERVIEW	102
5.8.1	GUIDING PRINCIPLES	102
5.8.2	PROPOSED SUSTAINABILITY GOALS	104
5.8.3	SUSTAINABILITY FRAMEWORK	104
5.8.4	SUSTAINABILITY STRATEGY OVERVIEW	106
5.8.5	STORMWATER MANAGEMENT INTEGRATION AND LINKAGES	110
5.9	STREETS	113
5.9.1	STREET HIERARCHY & ALIGNMENT	113
5.9.2	FINE-GRAIN STREETS & NEIGHBOURHOOD STRUCTURE	114
5.9.3	STREET TYPOLOGIES (RIGHT-OF-WAYS)	114
5.9.4	STREETSCAPE ELEMENTS	127
5.10	TRANSIT STRATEGY	128
5.11	COMMUNITY BENEFITS	130
5.12	PUBLIC ART	131
5.13	CULTURAL AMENITIES AND PROGRAMMING	133
5.13.1	CULTURAL PROGRAM	133
5.13.2	RETAIL PROGRAM	134

	Implementation	141
6.1	PHASING	141
6.2	SERVICING / INFRASTRUCTURE STRATEGY	142
	6.2.1 RECOMMENDED WASTEWATER / WATER SERVICING STRATEGY	142
	6.2.2 STORMWATER MANAGEMENT / STORM SERVICING	142
6.3	PARKING STRATEGY	146
6.4	TRAVEL DEMAND MANAGEMENT (TDM)	147
6.5	TRANSPORTATION INFRASTRUCTURE REQUIREMENTS	148
6.6	NEXT STEPS	149

Executive Summary

The Lakeview Village Development Master Plan sets a framework for the development of ‘Lakeview Village’, the Greater Toronto Area’s (GTA) most innovative, unique, and exciting waterfront community.

The Lakeview Village Development Master Plan (DMP) applies to the lands formerly owned by Ontario Power Generation (OPG), which are now owned by Lakeview Community Partners Limited (LCPL). This Plan is required by the City of Mississauga as a bridge between the policy planning framework in the City’s Mississauga Official Plan (MOP) and the eventual detailed development applications to be submitted for review and approval by the City. More specifically, the DMP builds on the legacy and vision of the Inspiration Lakeview Master Plan (ILMP). This DMP essentially is a continuation of the planning and design efforts from the past but now advances the project to develop and execute on the vision while still fulfilling the City’s MOP requirements.

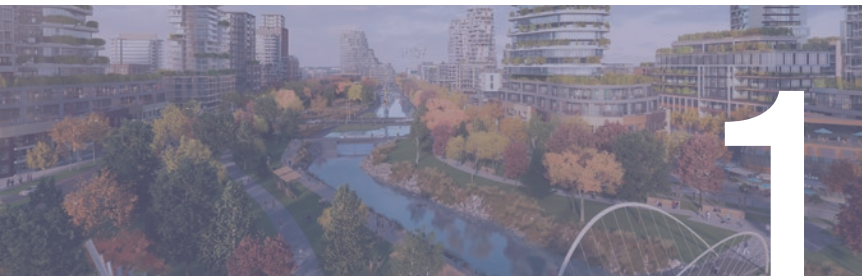
It is intended that this DMP will provide guidance for future land use planning and development application processes, recognizing that some of today’s underlying assumptions may change over time. This does not weaken the content or intent of the Development Master Plan, but rather directs LCPL to consider the broader context and overall area requirements as noted in other sections of this DMP and in other applicable approval authority documents. It is understood that with time, amendments may be pursued or required to this DMP and thus, the DMP has an element of fluidity for flexibility in the future.

Over the next year, LCPL will continue to work with the City of Mississauga to complete the necessary approvals for the site, further refine the strategies contained in this updated Development Master Plan, and begin the physical remediation process to prepare the site for redevelopment.



Figure i - North-west view of the existing Lakeview Village lands with downtown Mississauga in the distance

The Development Master Plan has been structured in the following manner:



1.0 INTRODUCTION

Section 1 describes the vision for Lakeview Village, the purpose of the DMP, a brief overview of the project to-date, and introduces the ownership group responsible for the project and consultant team leading the development process. It also addresses the role that the DMP will have in guiding the ongoing planning and design of future development within Lakeview Village.



2.0 BACKGROUND

Section 2 provides an illustrative overview and timeline of the site's history, the evolution of the project, and the ongoing community consultation process. This section also highlights the legacy of the late Councillor Jim Tovey and the continuation of his vision for Lakeview. A summary of the policy framework addresses the applicable regional and local planning documents, including the Official Plan Amendment (OPA), that have informed the DMP. A description of the key elements to the plan illustrates the evolution from the ILMP to the current Master Plan.



3.0 SITE CONTEXT

Section 3 describes the location of Lakeview Village at the regional, local, and site level. It provides a description of surrounding neighbourhoods, parks, transit services, key anchors, institutions, and major roads. The section also includes maps / diagrams to illustrate the site context and local amenities with corresponding images of existing site conditions.



4.0 STRUCTURING ELEMENTS

Section 4 provides a general overview of the structuring elements of the plan that serve as the building blocks to define the land uses, streets and blocks plan, and districts. The structuring elements for Lakeview Village include the original '6 Big Moves' from the Inspiration Lakeview Master Plan that have provided the framework for the Development Master Plan, including a continuous waterfront, a 'blue and green network', a fine grain street pattern, bringing transit to the site, a cultural hub at the head of the piers, and the employment / innovation corridor that will contribute to a healthy, complete, and sustainable community.



5.0 THE PLAN

Section 5 addresses the detailed aspects of the Development Master Plan, including the vision, districts and neighbourhoods, waterfront parks and open spaces, active transportation, retail, culture and employment uses, activation, development program, built form, streets, street hierarchy and alignment. It also provides a summary of the sustainability / green development standards, community benefits, affordable housing, public art, recreational facilities, cultural amenities, and all related programming.



6.0 IMPLEMENTATION

Section 6 describes how the Development Master Plan will be executed, including the mechanisms and processes by which this DMP document is to be implemented. This will include an approach to service / infrastructure strategy, parking strategy, travel demand management (TDM), environmental strategy process, and transportation infrastructure strategy. The report will conclude with a summary of next steps in the development of Lakeview Village following the DMP.



INTRODUCTION



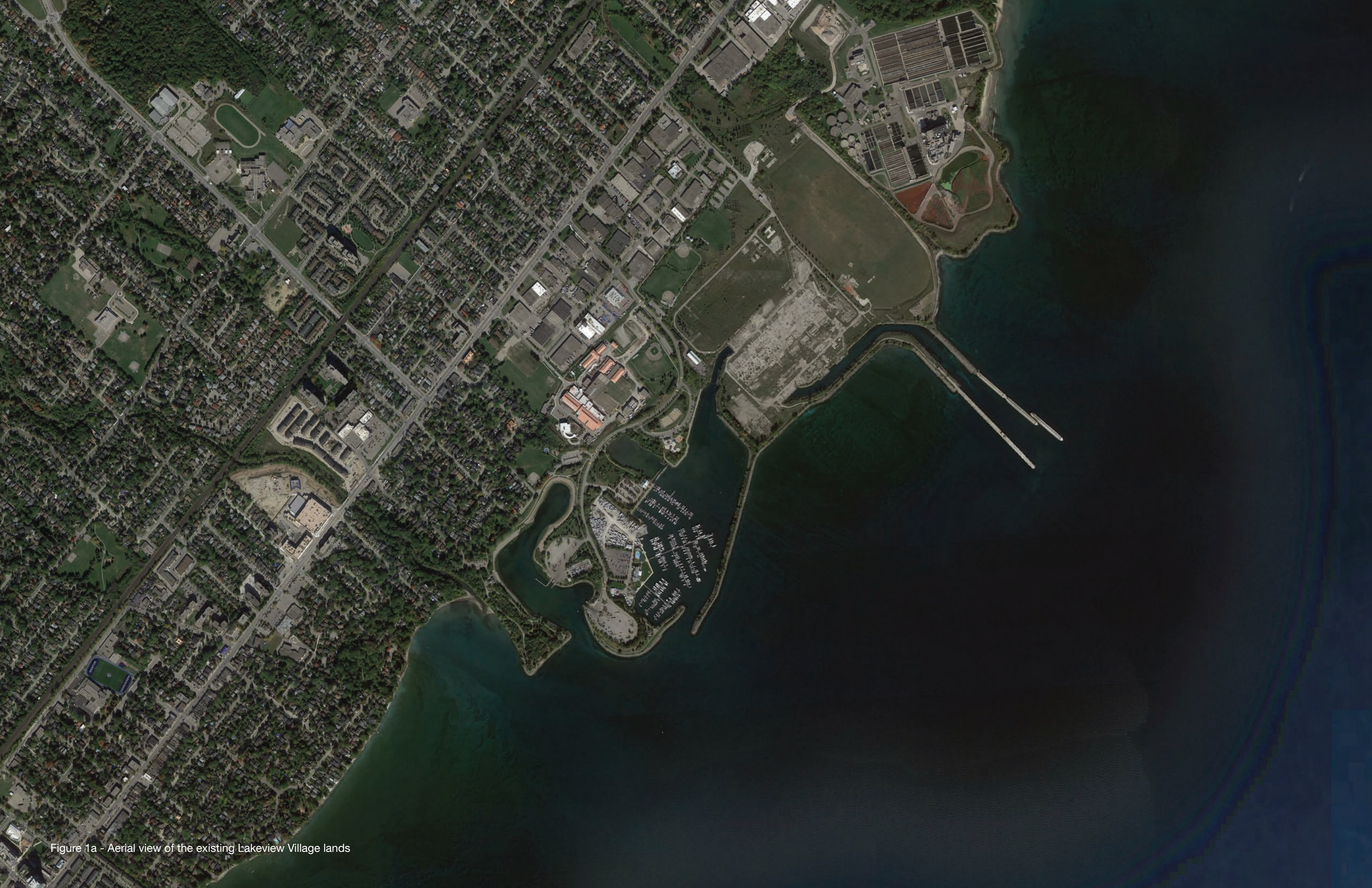


Figure 1a - Aerial view of the existing Lakeview Village lands

Introduction

Lakeview Village will create a place where people can connect with Lake Ontario. Delivered by Lakeview Community Partners Limited, this new community for the City of Mississauga will be a model in green and sustainable urban living.

Lakeview Village will breathe new life into Mississauga's waterfront, reconnecting the community back to the water's edge with a diverse mix of residential offerings, as well as institutional, cultural, office and retail spaces. Sustainability is the key foundation of this new community, both through its design, as well as the mix of uses, public spaces, and diverse programming that will be delivered at Lakeview Village. The project will support and contribute to local commerce, and will yield over 3,000 new jobs.

LCPL has worked in partnership with City Staff, City councillors, and local stakeholders to build on the 2014 Inspiration Lakeview Master Plan. This DMP respects the ILMP, maintaining its foundational elements, while incorporating updated market information and limited physical modifications to allow for the immediate development of the project. Throughout the process of refining this plan, LCPL and its team have maintained open dialogue with City staff and local stakeholders to ensure that the updated plan aligns with municipal policies and priorities, as well as the community vision for the site.



Figure 1b - Existing north-west view of the Lakeview Village lands



Figure 1c - View from Lakefront Promenade Park with the Lakeview Village site across the water

1.1 LAKEVIEW COMMUNITY PARTNERS LIMITED (LCPL)

LCPL is a partnership of Greater Toronto’s leading community builders that includes TACC Construction Limited, Greenpark Group, CCI Development Group, Branthaven Homes, and Argo Development Corporation.

LCPL has acquired the subject lands from Ontario Power Generation and are in the process of articulating a clear vision and strategy for the land. LCPL’s unified goal is to complete all of the necessary regulatory approvals required for the development and construction of buildings on the subject lands. This consortium will, in essence, be in control of the project from inception to execution of the final built product.

The LCPL consortium comprises the following developers:

TACC Construction Limited

Built on the principles of hard work, ingenuity, and a commitment to excellence, TACC Construction Limited is one of the largest municipal infrastructure contractors in Ontario. Since it was established by Silvio and Carlo De Gasperis in 1977, land developers and government stakeholders have relied on TACC Construction to deliver innovative and quality infrastructure work.

Greenpark Group

With a 50-year proven track record, Greenpark Group is the Greater Toronto Area’s comprehensive homebuilder, offering a full suite of new home designs from towns, semis, and singles to high-rise condominium residences. Since 1967, over 55,000 families have chosen Greenpark Group for its excellence in both design and construction.

CCI Development Group of Companies

CCI specializes in real estate development and investment activities in Canada. CCI is Canadian owned and controlled, and focuses on the acquisition, remediation, and sustainable redevelopment of properties with real or perceived environmental contamination in markets across the country.

Branthaven Homes

Branthaven Homes has reimaged the homebuilding industry with unique, design-oriented developments. Since its founding as a family business in 1971, Branthaven Homes has grown from a master builder of custom estate homes into one of Canada’s premier homebuilders and developers of master planned communities in the most desirable residential areas of GTA West and Southern Ontario.

Argo Development Corporation

Argo Development Corporation is a local, industry leading land development company with over 25 years of experience. Argo Development Corporation works diligently with its valued partners to create thriving communities in the most desirable locations across the GTA and South West Florida.

1.2 THE CONSULTANT TEAM

Supporting LCPL is a team of experts in architecture, planning, urban design, placemaking, and transportation engineering. Collectively, this team is working with LCPL to formulate and deliver the Lakeview Village vision.

The consultant team consists of the following firms:

Alberto Bicol Consulting Inc.	Building MEP and Energy Specialist
Cicada Design	Creative Media & Communications
FVB Energy	District Energy Specialist
Gerrard Design	Urban Design
Glen Schnarr & Associates Inc.	Urban Planning
LiveWorkLearnPlay	Real Estate Advisory
LRK	Architecture and Urban Design
NAK Design Strategies	Urban Design and Landscape Architecture
RWDI	Wind and Odour Specialist
Spanier Group	Mixed-Use Development Advisory
The Municipal Infrastructure Group	Sustainability, Transportation, and Civil Engineering
Urbantech Consulting	Civil Engineering
Urban Strategies Inc.	Community Consultation



BACKGROUND

2



Figure 2a - View of the Four Sisters' smokestacks at the shoreline of the former Lakeview Generating Station

Background

2

The 177 acre site, located in Mississauga's Lakeview community, east of Port Credit near Lakeshore Road East and Cawthra Road, is the former site of the Lakeview Generating Station, a coal-fired power plant that was operational from 1962 to 2005.

The station's four 146 metre smokestacks, known as the Four Sisters, became a familiar landmark along the Lake Ontario shoreline and could be seen from Burlington and downtown Toronto.



Figure 2b - The former location of the Lakeview Generating Station run by Ontario Power Generation.
Figure 2c (Circle) - Coal delivery at former Lakeview Generating Station

2.1 HISTORIC OVERVIEW

The Lakeview area has had a unique and diverse past that will be celebrated in the design and development of Lakeview Village. Its history ranges from early Iroquois First Nations, French and English settlements and farming plots, to a significant military and industrial presence in the 20th century, including until recently, the Ontario Power Generation Plant.

Since the 1900's, Lakeview's notable history includes both aviation and industrial milestones, with the Long Branch Aerodrome serving as Canada's first airport and aviator training school, later becoming the training centre for the Royal Flying Corps. The Small Arms Limited building played an important role during the Second World War, producing large quantities of military small arms almost exclusively by women. The original building still stands in close proximity to the site, on the south side of Lakeshore Road East.

The Lakeview area's distinct history shall be recognized and built upon as the area continues to adapt and adjust to reflect the needs of the City of Mississauga. The following historic overview highlights different aspects of the Lakeview lands transformation.

INDIGENOUS HISTORY

- The pre-contact years for Mississauga are divided into 3 distinct periods: Paleo-Indian (9000-8500 BC), Archaic (8000-1000 BC), and the Woodland Period (1000BC-AD 1650)
- Mississaugas of the New Credit Land Cessions
 - Head of the Lake, Treaty 14 (1806)
 - Treaty 22 (1820)

MILITARY HISTORY

- 1868 – Ontario Rifle Association
- 1891 – Long Branch Rifle Ranges
- 1915 – Canada's First Aerodrome
- 1917 – Royal Flying Corps using the Curtiss Aerodrome
- 1940-45 – Small Arms Ltd. (Department of National Defence)
- 1940 – Small Arms Training School, Militia Training Camp
- 1946-1954 – Lakeview Army Barrack's Emergency Housing
 - Temporary housing for returning soldiers and their families

INDUSTRIAL HISTORY

- 1940-45 – Small Arms Ltd.
 - Women in the workforce
- 1945-1974 Canadian Arsenals: Small Arms Division
 - Manufactured rifles and engine components
- 1951 – Canadian Admiral Factory (manufacturer of televisions)
- 1953 – Lakeview Water Treatment Plant
- 1957 – Lakeview Sewage Treatment Plant
- 1962 – Lakeview Generating Station

TRANSPORTATION

- Lakeshore Road opened in 1804 – historically used by the Mississauga from York to Niagara.
 - Became the first road to be designated a cement highway in 1914
- The Great Western Railway – 1855 – connecting Toronto to Hamilton and stopping in Lakeview
- 1916 – Electric Radial Cars along Lakeshore Road
- 1939 – Extension of TTC built short line to take workers from Long Branch to Small Arms Ltd.

Source: City of Mississauga Community Services Department / Heritage Mississauga



Figure 2.1a - The Lakeview lands and associated historical features



1. Small Arms Ltd., Administration Building, circa 1945



2. Women at Small Arms Ltd., circa 1940-1945



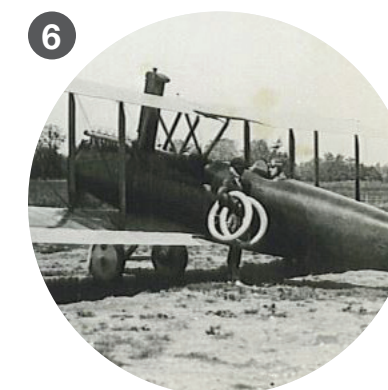
3. Canadian Admiral Factory, circa 1951



4. Lakeview Army Barrack's emergency housing



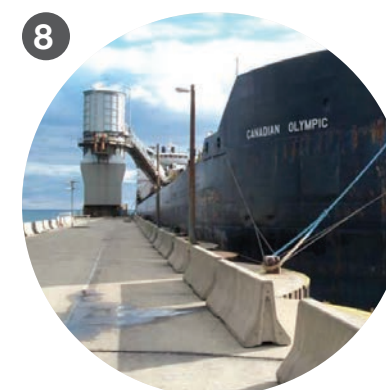
5. Long Branch Aerodrome, circa 1920



6. Image of Curtiss Airplane at Longbranch Aerodrome in 1917



7. View of the former Four Sisters' smokestacks



8. *Canadian Olympic* docked at former Lakeview Generating Station



9. Shoreline of the former Lakeview Generating Station

2.2 INSPIRATION LAKEVIEW & THE COMMUNITY PLANNING PROCESS

Following the closure of the Lakeview Generating Station and eventual decommission of the site, OPG and the City of Mississauga began to look towards the future and started planning how to best repurpose the lands in the public interest. A community grass-roots initiative conceived by the Lakeview Ratepayer's Association started an effort to envision a future for this area, which became known as the Lakeview Legacy Project.

The community led effort was considered at that time a first in Canada, where a community had spearheaded its own redevelopment vision for a brownfield location. In recognition of their pioneering effort, the Association was awarded a bronze medal in urban design from the Canadian Design Exchange Awards in 2009.

In 2011, an initial Memorandum of Understanding (MOU) was signed between the City and the Province outlining the common goals of site remediation and the redevelopment of the Lakeview site into the GTA's newest waterfront community. From the initial MOU, a substantial community planning process was launched by OPG and the City to solicit thoughts and ideas for how the new Lakeview community should be created. The engagement process resulted in the 2014 Inspiration Lakeview Master Plan completed by Urban Strategies. With ILMP document in place, the Province committed to assist in the remediation of the shoreline, and future public parkland was secured.



Figure 2.2a - Community update - November 2013



Figure 2.2b - Lakeview visioning - December 2010



Figure 2.2c - Community update - November 2013



Figure 2.2d - 'Crafting the vision together' - December 2010



Figure 2.2e - Community update - May 2014



Figure 2.2f - 'Crafting the vision together' - December 2010



Figure 2.2.1 - Inspiration Lakeview Master Plan - June 2014

2.2.1 IMPLEMENTING INSPIRATION LAKEVIEW

To implement the development plan outlined in Inspiration Lakeview, the City of Mississauga and OPG issued a joint Request for Proposals to the private sector; soliciting interest and refined plans from interested developers. Lakeview Community Partners Limited was selected as the winning proponent in late 2017 to develop the lands in accordance with the principles and vision of the Inspiration Lakeview Master Plan.



Figure 2.2.2 - Councillor Jim Tovey at the Lakeview Pier

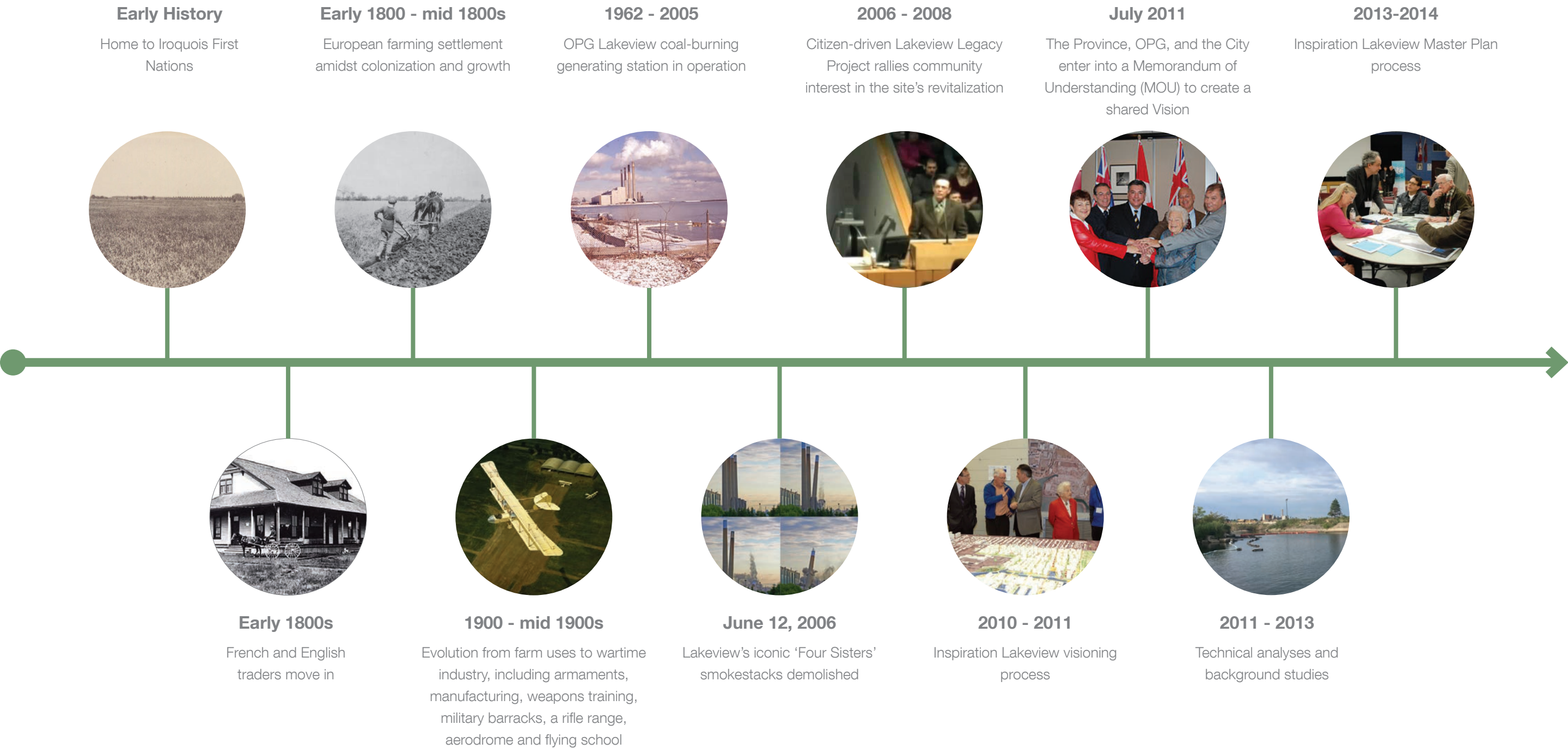
2.2.2 THE JIM TOVEY LEGACY

The late Councillor Jim Tovey was instrumental in guiding the new vision for the southern part of the Lakeview community as the leader of the Lakeview Ratepayer's Association. In this role, and with all his efforts for the ILMP, the City of Mississauga recognized him as Volunteer of the Year in 2009. His passion for the ILMP continued as an area Councillor by leading the City to continue the effort from the ILMP and review the City's policy planning framework. He also spearheaded many local initiatives resulting in funding contributing to advancing the ILMP vision. Lakeview Community Partners Limited was fortunate enough to introduce the Councillor to the proposed plans in late 2017, which he was excited to see, as it shaped his many years of community work into reality.

Recognizing Councillor Tovey's pivotal role in this project, in June 2018 Credit Valley Conservation (CVC) announced that the conservation area being constructed along the Mississauga waterfront will be named in his honour.

While the community and area stakeholders have lost a true champion, LCPL and its team are committed to working collaboratively with all stakeholders, including the local community, the City and Region, and other approval authorities to honour Councillor Jim Tovey's vision for a vibrant, sustainable, and connected community on the shores of Lake Ontario.

2.2.3 TIMELINE OF PROJECT & MASTER PLANNING PROCESS



Timeline Source: *Inspiration Lakeview*



2.3 LAKEVIEW VILLAGE COMMUNITY RE-ENGAGEMENT

While developing this DMP, LCPL, in collaboration with Urban Strategies, re-engaged local stakeholders and the Mississauga community through a series of community meetings and outreach sessions. The meetings were an opportunity to present the updated plan to the community and gather feedback and suggestions to improve upon the extensive work in process by LCPL.



Figure 2.3.1a - 2.3.1c - Community re-engagement event - April 18, 2018

First and last name (optional):

Address (optional):

Email and phone number (optional):

01

How supportive are you of Lakeview Village? (1= not at all supportive, 5 = extremely supportive)

1 2 3 4 5

02

What is your number one priority when it comes to the redevelopment of the Lakeview site?

03

Please rate the usefulness of the information you received. (1= not at all useful, 5 = extremely useful)

1 2 3 4 5

04

What would you like to hear more about?

05

Please rate how effective you found today's format. (1= not at all effective, 5 = extremely effective)

1 2 3 4 5

06

Based on your experience, how might we improve these meetings in the future?

07

Other questions and comments:
If you are submitting a question, please ensure you have provided your name and contact details above. (Continue on back)

08

Would you like to be added to Lakeview Community Partner's mailing list? YES NO
If yes, please ensure you have provided your name and contact details above.

09

Would you like to receive notifications for future meetings? YES NO
If yes, please ensure you have provided your name and contact details above.

Responses:

	Average Score	Lowest Score
Question 1: How supportive are you of Lakeview Village?	4.83	4
Question 3: Please rate the usefulness of the information you received	4.1	3
Question 5: Please rate how effective you found today's format	4	3

Source: Urban Strategies

Figure 2.3.1d - Community re-engagement questionnaire

2.3.1 COMMUNITY RE-ENGAGEMENT
EVENT - APRIL 18, 2018

LCPL hosted a community re-engagement event on April 18, 2018 with the goal of reaching out to the Lakeview community to gather feedback on the updated vision and plan for Lakeview Village. Approximately 120 members of the community and City staff attended the event. Following an overview presentation, participants spent an hour and a half circulating between five breakout stations, organized around the themes of ‘Districts/Programming’; ‘Built Form/Height’; ‘Transportation’; ‘Sustainability’; and ‘Parks and Public Realm’.

Overall feedback from the session was generally positive and attendees were supportive of the project moving ahead. Many people contributed ideas for programming, site activation, transportation planning, built form, and open spaces. Some attendees expressed concerns, and others support, for building heights and density, as well as the locations of both. Participants expressed interest in learning more about the project’s various components, such as: phasing and implementation; traffic, transit and active transportation; site remediation; waste collection; and public spaces. Attendees also expressed interest in learning more about the progression of the project and inquired about future consultation and engagement.

SUMMARY OF FEEDBACK FROM PARTICIPANTS

General Comments:

- Lots of specific programming and site activation suggestions
- Many ideas related to transportation planning (e.g. signage, bollards)
- Interest in commemorating Jim Tovey
- Specific built form and open space layout-related suggestions
- Desire to see Indigenous communities reflected in the plan (e.g. through engagement and programming)

Areas of Concern:

- The most common concerns were related to building heights and density, and the locations of both
- Some participants expressed making Lakeview Village an area that is affordable and accessible to both residents and visitors as a main priority
- Planning for active transportation and transit, and avoiding congestion
- Need for additional/larger cultural space
- Concerns related to garbage collection, wastewater circulation, and wind

General Support For:

- The overall project
- Moving ahead with implementation
- Proposed mix of land uses
- Format of engagement session

Topics Participants Want to Learn More About:

- Progress of the project, phasing, implementation plan, consultation, and remediation process
- Traffic and transit
- Public green and open spaces
- Amenities

2.4 POLICY CONTEXT

The Province, OPG, City, and local community worked together to develop a shared vision for the former Lakeview Generating Station site, resulting in the Inspiration Lakeview Master Plan. The plan calls for the brownfield site and surrounding employment lands to transform into a mixed-use community with a variety of residential building types, parkland, and cultural and employment uses, with considerations for environmentally sustainable site features and designs.

2.4.1 OFFICIAL PLAN POLICIES

The City of Mississauga has recently completed a comprehensive review of the Official Plan for the employment area of the Lakeview District. This policy planning exercise was a compilation of the City’s ongoing cycle of Official Plan reviews, including implementation of the recently-completed Municipal Comprehensive Review work, as well as creating the policy framework to implement the vision and key principles as detailed in the ILMP. The process formally commenced when the City released its first draft of the Lakeview Waterfront Major Node Character Area policies during the December 5, 2016 public meeting.

Lakeview Waterfront Major Node Character Area

With ongoing public and landowner feedback, and technical considerations by internal departments and external agencies, City staff have revised the policies with versions of the document published in January 2018, May 2018, and finally June 2018.

The City’s final report including public comments on the proposed Lakeview Waterfront Major Node Character Area Policies was dated June 11, 2018 and was presented to the City’s Planning and Development Committee on June 25, 2018. At the Committee meeting, local Councillor Dave Cook brought forward a motion requesting minor changes to some of the policy framework which focused on the mixed-use focal point in the southeast part of the community, development application processing, and community engagement. LCPL deputed at the Committee meeting noting full support for the revised Official Plan framework and content of Councillor Cook’s motion. The staff report, including amendments through Councillor Cook’s motion, was unanimously approved by the Committee, resulting in approval by City Council on July 4, 2018.

The appeal period for the revisions to the Mississauga Official Plan (OPA 89) was cleared on July 31, 2018 and the policy revisions are now in full force and effect for the LCPL lands.

Lakeview Village Lands - Applicable Policies

Since the Lakeview Village lands are part of the Lakeview Waterfront Major Node Character Area, the following is noted to highlight specific policy context relevant to the site:

- General policies have been prepared, providing a vision which carries forward the ILMP guiding principles;
- General policies also include references to guiding growth, valuing the environment, creating complete communities, amongst other high-level subjects;
- Each precinct in the Major Node has a unit target as well as a built form distribution;
- Of the four character area precincts in the Major Node, one is partially, and two are exclusively within the limits of the LCPL lands, the City refers to these precincts as: Ogden Village, Cultural Waterfront, and Innovation Corridor;
- Site specific land use policies including built-form height allowances and flexibility for some additional building height, land use compatibility, and overall use provisions are in the MOP. Specific policies are referenced in Section 5.6 and 5.7 of this DMP;
- Details regarding area-wide and specific precinct study requirements are noted as part of development application review, processing, and approvals. This includes the requirements for this Lakeview Village Development Master Plan.

2.5 KEY UPDATES TO THE PLAN

The Lakeview Village Master Plan retains the key foundational elements of the Inspiration Lakeview Master Plan, including a commitment to sustainable design and a comprehensive network of public space, providing continuous public access to the waterfront.

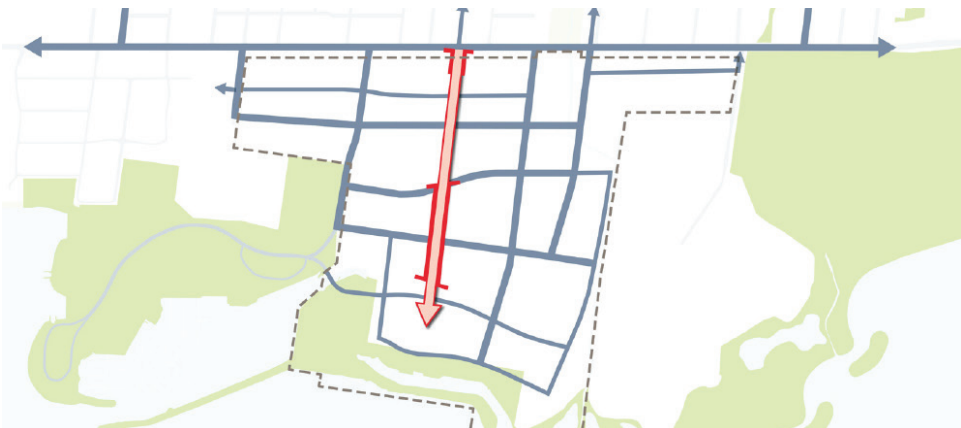
In addition to maintaining the overall vision and foundational elements established in the ILMP, the updated plan retains much of the proposed street grid, density targets, and mix of residential, cultural, and employment uses. The updated plan also conveys the same acreage (67 acres) of remediated waterfront land to the City of Mississauga.

As a result of feedback from the public consultation process, the key change to the plan is the shift of commercial and cultural opportunities to be closely aligned with the square, waterfront and future employment campus. By expanding on ideas to reinforce the place-making strengths of the central square, and shifting it closer to the waterfront and non-employment uses / cultural opportunities on the site's eastern edge, the updated plan improves upon ILMP's original design for the site's Cultural Hub.

INSPIRATION LAKEVIEW MASTER PLAN (ILMP) - 2014



Continuous Public Access to the Waterfront



Proposed Street Grid



Proposed Central Square

LAKEVIEW VILLAGE MASTER PLAN - 2018



Continuous Public Access to the Waterfront



Proposed Street Grid - Retains major collector roads proposed in the ILMP, with refined fine-grain street pattern.



Proposed Central Square - Updated plan has shifted commercial and cultural opportunities to be closely aligned with the square, waterfront, and future employment campus.

SITE CONTEXT

3



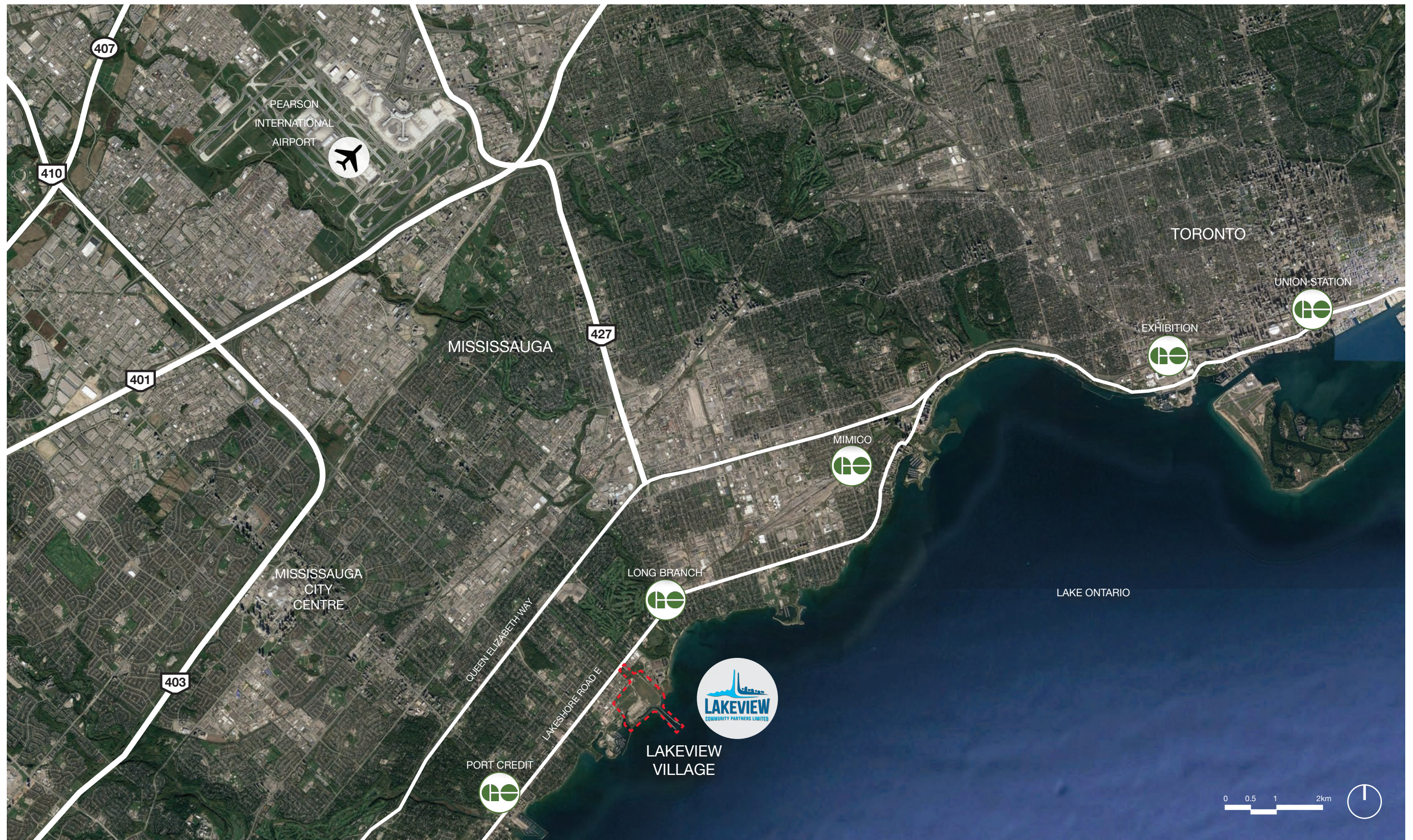


Figure 3a - Lakeview Village regional context and site location

Site Context

3

Lakeview Village is located in south-east Mississauga on Lake Ontario, approximately 3.8 kilometres east of Port Credit, near the western limits of the City of Toronto. The site is well-connected to major roads and highways, with Lakeshore Road East forming the north boundary of the site, and convenient access to both the Queen Elizabeth Way (QEW) and Highway 427.

Within the City of Mississauga context, Lakeview Village is located approximately 7 kilometres from the City Centre (Hurontario St. and Burnhamthorpe Rd.). To the west of the site, Hurontario Street provides a direct connection to the City Centre from Lakeshore Road.

Regional transit in the vicinity of Lakeview Village includes Long Branch GO Station and Port Credit GO Station, providing access to Hamilton, Oakville, Burlington, and Toronto via the Lakeshore West GO line. Local transit includes bus services along Lakeshore Road via MiWay (Mississauga Transit) and TTC (Toronto Transit Commission) streetcar and bus service departing from the Long Branch loop. Additionally, Lakeview Village's proximity to Pearson International Airport provides international accessibility to this future waterfront community.

Major anchors in the area include retail destinations such as CF Sherway Gardens and Square One. Institutions that also serve as major anchors include Humber College, Sheridan College, and the University of Toronto Mississauga.



Figure 3b - Lakeview Village local context and site location

3.1 EXISTING SITE FEATURES

The existing Lakeview site and immediate surrounding lands consist of largely light industrial uses on Mississauga’s waterfront, including two regional infrastructure facilities. Extensive active and passive recreational parkland exists within and around the development lands. The Great Lakes Waterfront Trail runs through the north end of the site, where it will ultimately be shifted along the water’s edge to form a continuous link that will provide cyclists and pedestrians access to Lakeview Village’s future amenities and services.

North: Lakeshore Road East, a significant arterial road running through Mississauga, borders the northern-most end of the site. An established residential neighbourhood is located north of Lakeshore Road, and is characterized by mostly low-density built forms with some higher density forms near Lakeshore Road East. Business employment lands are located south of Lakeshore Road, consisting of predominantly single storey commercial / light industrial buildings.

East: The G.E. Booth Wastewater Treatment Facility is situated immediately east of the site, with Lakeshore Park / Marie Curtis Park located further east.

South: The shores of Lake Ontario frame the south boundary of the site.

West: Immediately west of the site is the Lakeview Water Treatment Facility and Lakefront Promenade Park. Recreational amenities include the Port Credit Yacht Club, a public marina, the Mississauga Sailing Club, and the Waterworks Baseball Diamond.

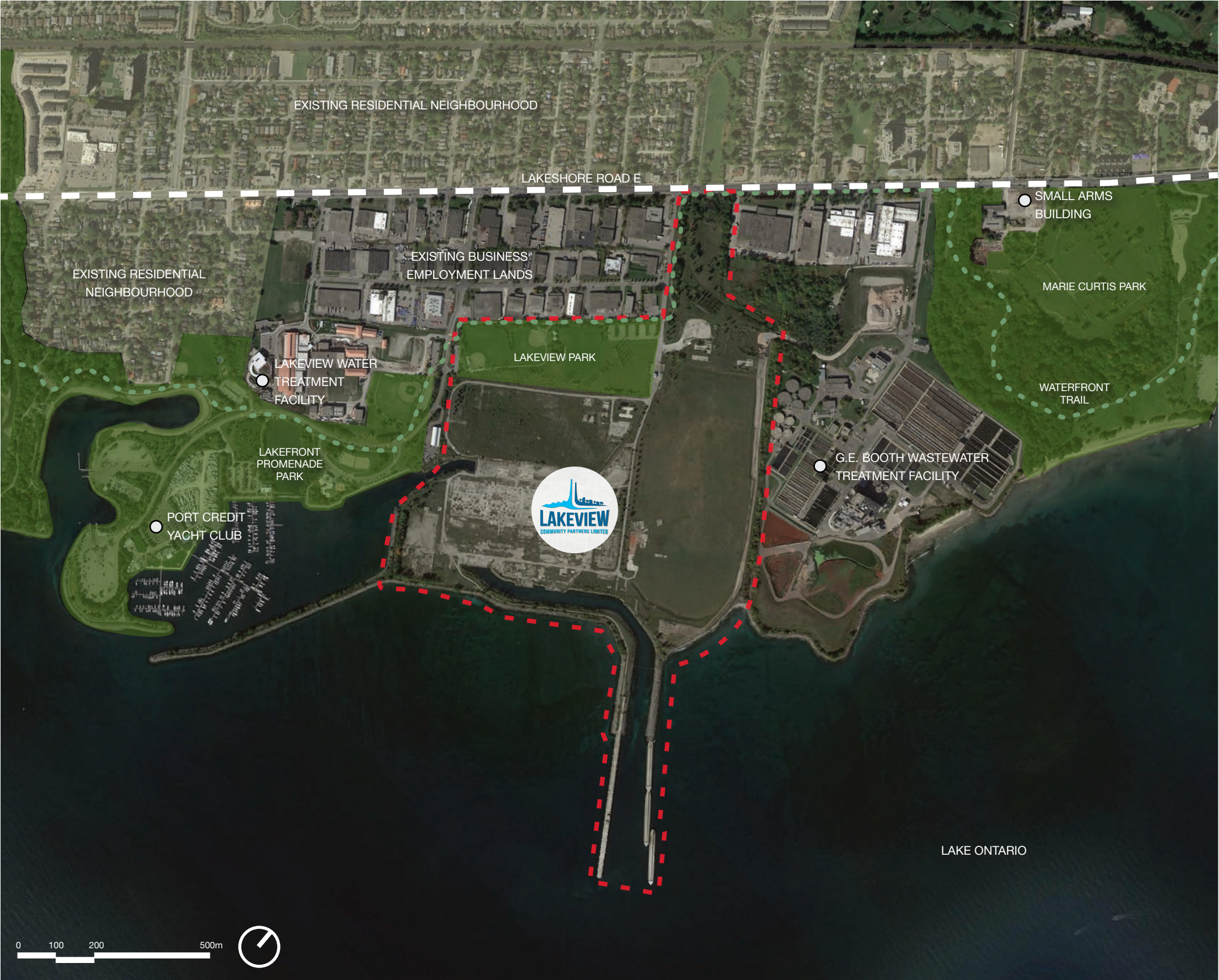
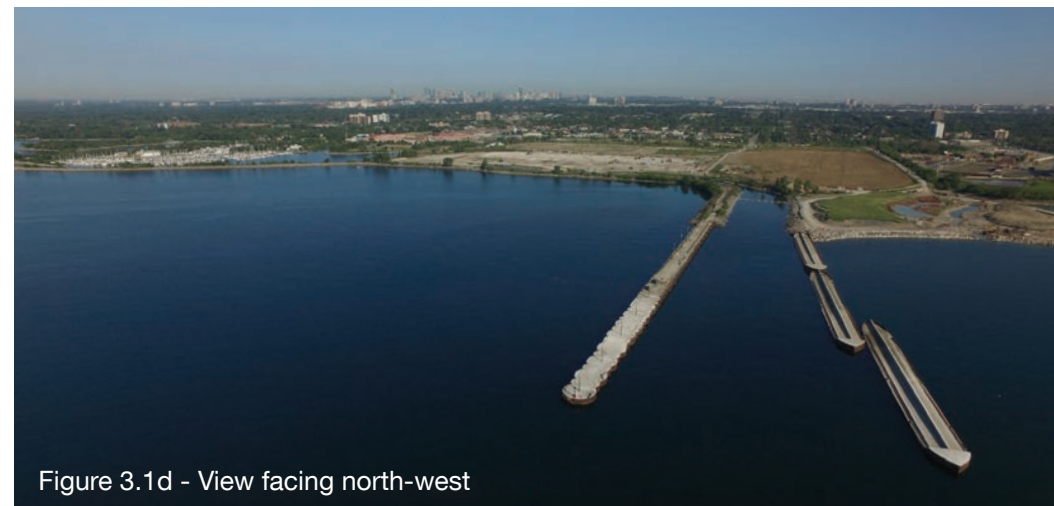


Figure 3.1a - Lakeview Village site context and location





STRUCTURING ELEMENTS





Figure 4a - Aerial image of the Lakeview Village lands and the Mississauga shoreline

Structuring Elements



4.1 THE '6 BIG MOVES'

The identification of a set of key structuring principles known as the '6 Big Moves' was established at the outset of the Inspiration Lakeview Master Plan development process. These key principles have been adopted to continue to inform the development of the proposed DMP, providing the structuring framework and organizing elements for the configuration of streets, districts, neighbourhoods, and associated land uses.

This section provides a general description of the following '6 Big Moves' and how they have been used to structure the proposed community:

- A Continuous Waterfront
- A Blue & Green Network
- A Fine Grain Street Pattern
- Bringing Transit to the Site
- A Cultural Hub at the Head of the Piers
- Employment & Innovation Corridor



Figure 4.1a - A Continuous Waterfront

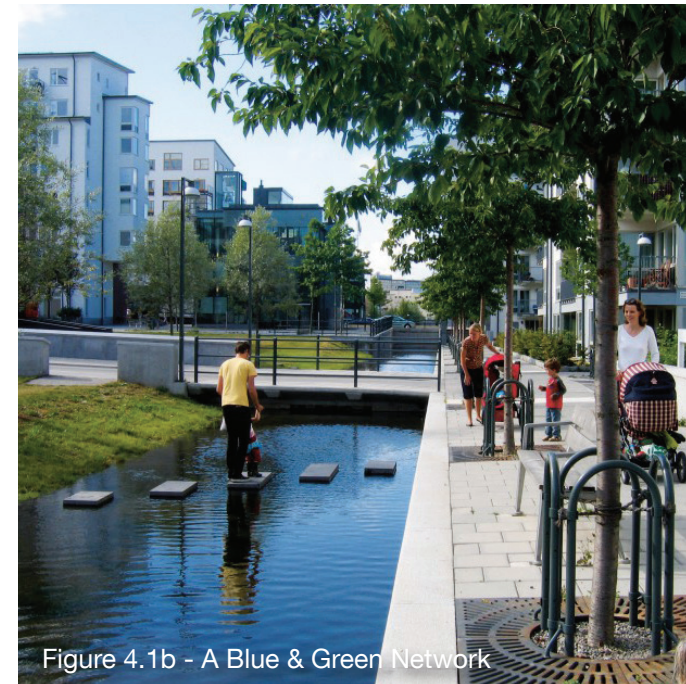


Figure 4.1b - A Blue & Green Network



Figure 4.1c - A Fine Grain Street Pattern



Figure 4.1d - Bringing Transit to the Site



Figure 4.1e - A Cultural Hub at the Head of the Piers

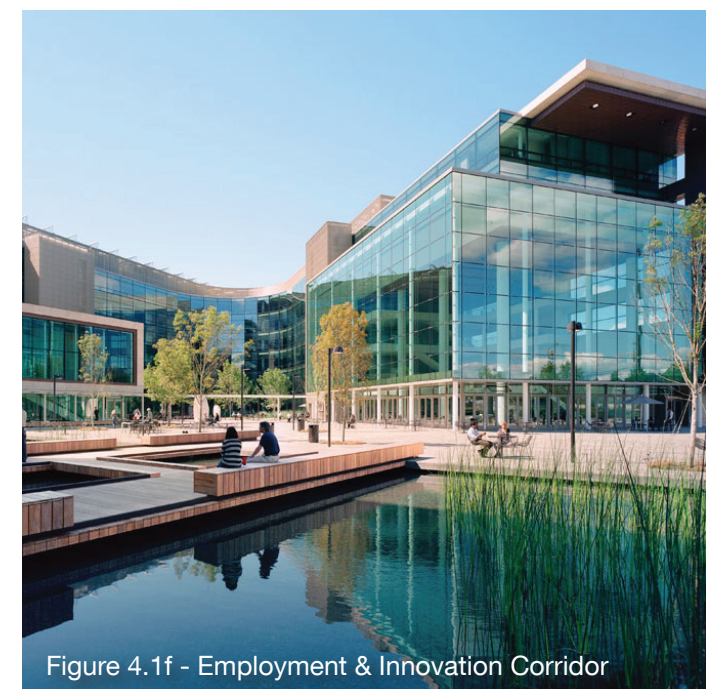


Figure 4.1f - Employment & Innovation Corridor

4.2 A CONTINUOUS WATERFRONT

A continuously linked waterfront open space system is at the core of the vision for the Lakeview Village, providing an uninterrupted water's edge connection from east to west, linking with existing park systems on both sides with the new waterfront amenity and the emerging Jim Tovey Lakeview Conservation Area immediately to the east.

A key component of achieving the continuous connection is the linking of the existing Waterfront Trail to the east and west of Lakeview Village, resulting in a complete and improved recreation trail integrated along the shore of Lake Ontario.

The plan conveys 67.1 acres of land to the City of Mississauga. Much of this remediated land will be converted into a new waterfront park, with multimodal trails that will form part of the Waterfront Trail, and active waterfront spaces. The plan protects public access along the waterfront throughout the length of the property.



Figure 4.2 - A continuously linked waterfront open space system

4.3 A BLUE & GREEN NETWORK

In addition to new public spaces along the waterfront, the plan includes a mix of public and open spaces that connect various neighbourhoods throughout Lakeview Village and provide important stormwater management functions.

A comprehensive approach to the layering of parks and open space features provides a robust network of green and water related public and private outdoor spaces that result in significant north-south and east-west linkages throughout Lakeview Village. The integration of low-impact development (LID) stormwater management features will form a key part of the blue network.

Linkages will comprise a variety of open space features and elements, including a hierarchy of park types, neighbourhood courtyard and mews conditions, character streets, and associated stormwater management functions. These will combine to form pedestrian and cycling connections, as well as view corridors, that deliver a network of distinctive cultural, multi-functional open spaces with integrated innovative sustainable (LID) features.

This approach achieves a core principle of the community which is connectivity, particularly north-south connections, linking the entire Lakeview community and beyond to the waterfront and other key character districts and neighbourhoods identified within Lakeview Village.



Figure 4.3 - A blue and green network of public and open spaces

4.4 A FINE GRAIN STREET PATTERN

The proposed street network is designed to allow people using various modes of travel (i.e. pedestrians, cyclists, transit riders, vehicles) to access Lakeview Village and move through the site safely.

Both as a means of structuring the community and providing the building blocks for distinctive districts and neighbourhoods, establishing a fine grain street pattern will appropriately respond to a multitude of users and functions. Ensuring all districts and neighbourhoods are well-interwoven by the street network is fundamental to ensuring pedestrians, cyclists, transit riders, and drivers have appropriate means to make direct, efficient, safe, and memorable connections throughout and to the water's edge.

Achieving street patterns that limit block lengths, reduce vehicular speeds, and adds to the character of Lakeview Village will promote walkability and is an important means of achieving a significant active transportation network that reduces reliance on vehicular travel within the community.



Figure 4.4 - A fine grain street pattern optimized for cyclists, pedestrians, transit riders, and vehicles

4.5 BRINGING TRANSIT TO THE SITE

Ensuring efficient and convenient transit options are provided to and from Lakeview Village is a fundamental component of the transportation and sustainability strategy. Lakeview Village is ideally situated in proximity to the Long Branch and Port Credit GO stations, future Hurontario Street LRT, and TTC transit hub, bringing residents, employees, and visitors within easy reach of local and regional destinations.

At this stage, it is anticipated that the transit link into Lakeview Village and the Employment and Innovation Corridor will bring local bus service along collector streets with direct connections to the two GO stations and a link to the future Lakeshore Road East transit facility.

Bringing transit to the site will be important for ensuring the long term sustainability of the project. The plan is designed to be flexible, so that transit can be incorporated as the project is phased and as regional transit plans are implemented.

Beyond traditional bus transit methods, new technologies and initiatives are presenting alternative options that focus on first and last mile issues and have recently emerged as real considerations for new community development. These include micro transit options, shared private services (such as uberPool or Lyft), and even autonomous vehicle services. Regardless of the ultimate method, the focus will remain on bringing a transit model that will see a significant increase in the modal split to transit and away from private car use.



Figure 4.5 - Bringing transit to the site and ensuring efficient and convenient transit options

4.6 A CULTURAL HUB AT THE HEAD OF THE PIERS

Arts, culture, retail, and public space will come together at the head of the piers. The plan concentrates a mix of activity-generating uses together, encouraging visitors to spend more time at Lakeview Village and enjoy many different experiences throughout the year.

The proposed cultural hub will become a dynamic, animated, and activated focus for Lakeview Village. It will combine a multitude of cultural venues and programming, indoor and outdoor, with retail opportunities, residential density, unique open space, and streetscape elements. Anchored by Lakeview Square, the cultural hub is strategically located with direct connections to the water's edge and associated programming, and to supportive uses such as the proposed Waterway Common and Serson Campus.

The Lakeview DMP has maintained the general location of the cultural hub but has further reinforced its potential success by surrounding it with complementary uses to ensure it becomes a vibrant anchor within the community and for Mississauga's waterfront.



Figure 4.6 - A cultural hub at the head of the piers includes arts, culture, retail, and public spaces

4.7 EMPLOYMENT & INNOVATION CORRIDOR

Employment and innovation are an essential part of the mix of uses in Lakeview Village. Serson Innovation Corridor is designed to support a mix of office, institutional, and innovation uses that will complement the planned residential, cultural, and retail uses as well as enhance the complete community in Lakeview Village.

The proposed Employment and Innovation Corridor provides the opportunity to strategically integrate a variety of employment uses (tech industries, office, light industrial) and potential education facilities within a sustainably focused district. As a transition area between proposed residential neighbourhoods and the existing G.E. Booth Wastewater Treatment Facility, the corridor will be well integrated into the urban fabric of Lakeview Village with a synergistic relationship to Lakeview Square and the surrounding retail and cultural amenities.

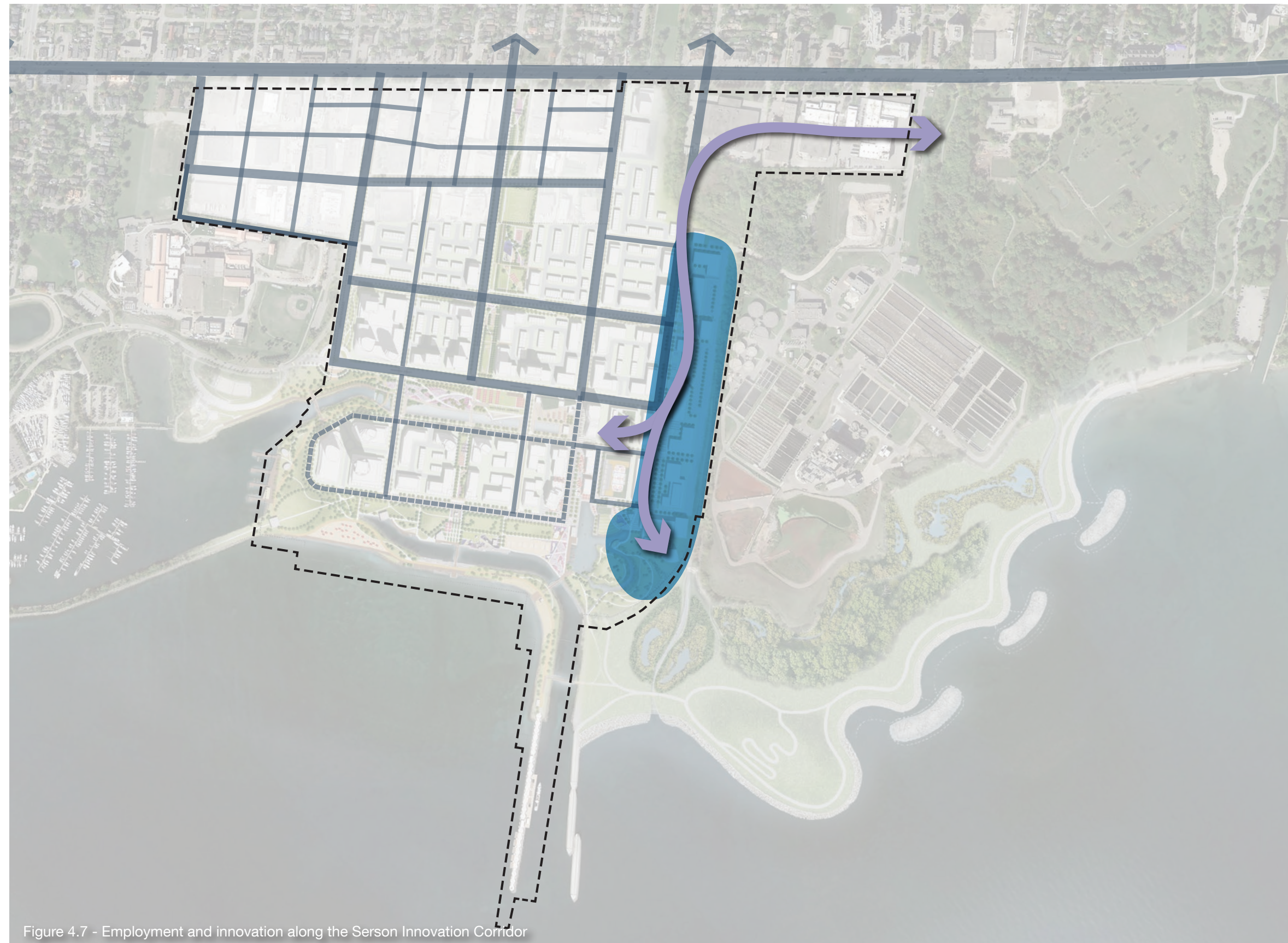


Figure 4.7 - Employment and innovation along the Serson Innovation Corridor

4.8 PLAN COMPARISON

Inspiration Lakeview and the comprehensive publicly driven process by which it was derived continues to be the foundation for the structure and vision of Lakeview Village.

As with the ‘6 Big Moves’, Lakeview Village has adopted the key guiding principles that have been identified by the City of Mississauga’s Official Plan for the Lakeview lands that are driven by the following:

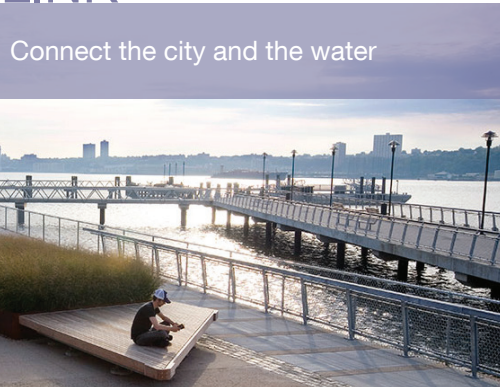
The adoption of the key guiding principles has resulted in the development of the Lakeview Village Master Plan that remains consistent with the vision, intent, and structure of Inspiration Lakeview’s outcome.

There are two significant moves that will bring additional value to these lands and reinforce a more complete community through effective placemaking and open space connections. These include the integration of Lakeview

Square with the adjoining Waterway Common and Inspiration Point as a major focus of the community and in support of the waterfront and its programming. The result of shifting this focus has enabled the plan to merge retail, cultural and employment amenities with new residential density all in close proximity to the waterfront while also providing a supportive relationship with the campus lands. This update to the master plan achieves the critical mass to support all uses.

LINK

Connect the city and the water



OPEN

Open the site with accessible public space for all



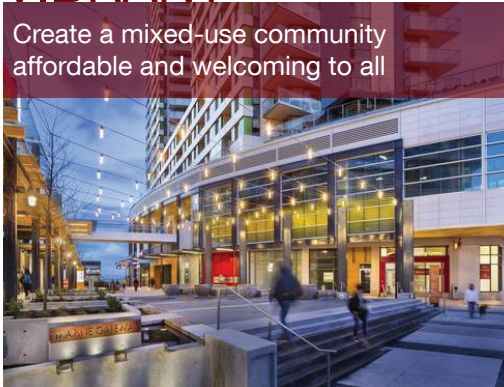
GREEN

Create a green, sustainable innovative model community



VIBRANT

Create a mixed-use community affordable and welcoming to all



CONNECT

Provide multiple ways to get around: transit, walking, & cycling



DESTINATION

Create a special place to draw visitors



REMEMBER

Commemorate history while creating a new legacy



VIABLE

Balance public & private investment to be economically sustainable



THE PLAN





Figure 5a - Preliminary Master Plan concept of Lakeview Village

5.1 VISION

Lakeview Village will become the Greater Toronto Area's most unique, innovative, and exciting waterfront community, reconnecting Mississauga to Lake Ontario.

A sustainable and diverse mix of residential offerings will allow people the unique opportunity to live and play on the lake. Institutional, cultural, office, retail, and amenities will draw everyone to Lakeview Village. Unique gathering and recreational spaces weave throughout the community contributing to a dynamic, walkable, connected, and active lifestyle at the water's edge and extending onto the lake at Inspiration Point.



Figure 5.1 - Lakeview Village Land Use Plan



Figure 5.2a - Preliminary view of Lakeview Village's southern districts

5.2 DISTRICTS & NEIGHBOURHOODS

The Lakeview Village Master Plan is organized into a series of coordinated and interconnected neighbourhoods or districts.

Each neighbourhood is characterized by an individual sense of place that collectively contributes to the project’s overall vision and experience. This neighbourhood strategy was developed based on market conditions, site constraints and opportunities, and the overall project vision and goals. These districts help to organize and concentrate activity and can also be useful in determining project phasing. Districts will be connected through the planned street network, trail connections and stormwater management facilities. Like all great places, the neighbourhood strategy includes built-in flexibility to evolve over time as the project develops.



Figure 5.2b - Conceptual Master Plan with districts overlay



Figure 5.2.1a - Preliminary concept of Lakeview Square

5.2.1 LAKEVIEW SQUARE

The heart of the Village, Lakeview Square will include a curated mix of ground floor specialty retailers; handpicked and each celebrating the unique character and leading design of best-in-class, non-national tenants.

Above grade residential or office uses will add additional vibrancy and help to frame the square. Lakeview Square will act as a destination, drawing people of all ages and backgrounds, encouraging them to stop and spend time in Lakeview Village. Adjacent to Waterway Common and Serson Innovation Corridor, the Square will also be home to a significant cultural hub, one that incorporates multicultural programs, specialty uses, and waterfront attractions throughout the public space. This space will draw on the diverse background and experiences of residents within the City of Mississauga to create a unique destination at Lakeview Village.



Figure 5.2.1b - Seasonal farmers market



Figure 5.2.1c - Cafe



Figure 5.2.1d - Location of Lakeview Square district



Figure 5.2.1e - Pedestrian priority



Figure 5.2.1f - Human-scaled development, public arts, and cultural performances



Figure 5.2.1g - Restaurants with patios



AVIATOR GREENWAY

AVIATO

LAKEFRONT PROMENADE

NEW OGDEN AVENUE

OGDEN GREEN

HYDRO ROAD

WATERWAY STREET

WATERWAY STREET

WATERWAY COMMON

SERSON PROMENADE

OGDEN VISTA PARK

LAKEFRONT BOULEVARD

Figure 5.2.2a Preliminary concept of Waterway Common

5.2.2 WATERWAY COMMON

Waterway Common provides the central gathering space for the community that links the existing park system to the west with the square.

Providing spaces for activity as well as quiet reprieve and defined by mid and high-rise development, Waterway Common will be a year-round public gathering place for residents and visitors alike. It will also provide a strong visual connection to Lake Ontario to the west and an important sense of arrival to the development in the early years of the project. Designed from the ground up as a four-season, outdoor space and water amenity, connected to residential living on the park and the animated Square to the east, Waterway Common will come alive each season with summer splash pads, spring orchards, winter skating rinks, and seasonal markets for everyone to experience and enjoy.



Figure 5.2.2b - Outdoor green and unique programming connected to residential living



Figure 5.2.2c - Location of Waterway Common district



Figure 5.2.2d - Skating rink and winter activation



Figure 5.2.2e - Fragrant and wildflower gardens



Figure 5.2.2f - Parkside living with distinctive architecture framing the edges



Figure 5.2.3a - Preliminary concept of Inspiration Point

5.2.3 INSPIRATION POINT

Once a former industrial pier, Inspiration Point will become one of Lakeview Village’s most inviting and celebrated features.

Extending more than 600 metres into Lake Ontario, this destination attraction will provide people from across the region an unparalleled vista of the waterfront experience. Programmed with public art, cultural pop-ups, and active public space along both the reclaimed pier and newly created park space, Inspiration Point will integrate seamlessly with Waterway Common and Lakeview Square. Providing unprecedented views to downtown Toronto, Mississauga, and across the entire Lake Ontario.

Further contributing to Inspiration Point’s cultural dynamism will be a civic, open-air music venue, built into the edge of ‘Coal Hill’ and programmed to let boaters, beach goers, and picnickers alike take in some of the region’s best acts.

Offering mid-rise housing with lakefront views, Inspiration Point will be defined by its significant public lakefront park, reconnected Waterfront Trail, and active transportation links that embrace the shoreline.

This district presents an opportunity to deliver unique architectural design and residential living that can be enjoyed by all, including visitors who come to visit Lakeview by water, by foot or by car to explore.



Figure 5.2.3b – Inviting places and spaces to stroll, sit, and enjoy

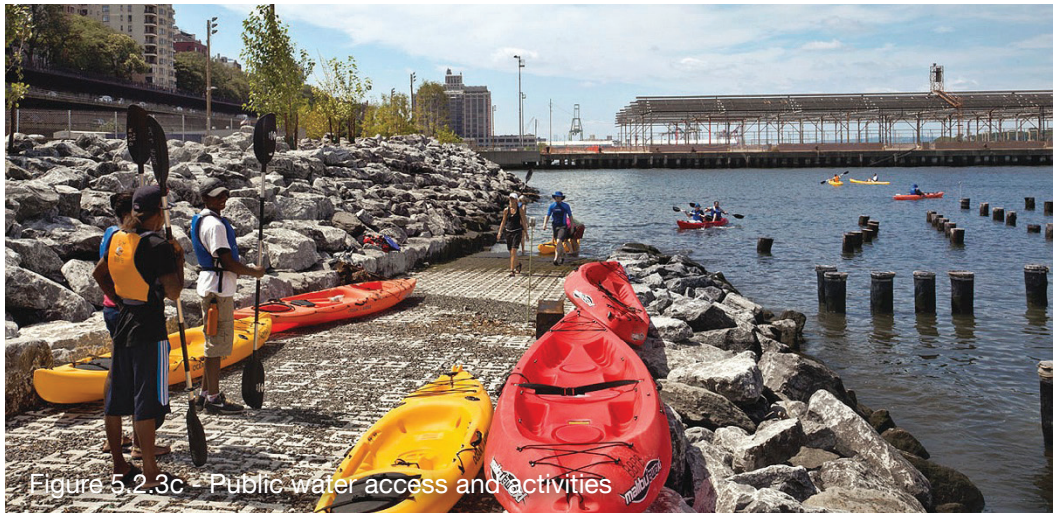


Figure 5.2.3c – Public water access and activities



Figure 5.2.3e – Temporary and interactive installations



Figure 5.2.3f – Themed public art



Figure 5.2.3d – Location of Inspiration Point district



Figure 5.2.3g – Variety of water interface conditions

5.2.4 SERSON INNOVATION CORRIDOR

Delivered through a partnership between the City of Mississauga and the Province of Ontario, Serson Innovation Corridor will be a model of innovative city-building; seamlessly offering office and next-generation employment uses alongside the Village’s residential, retail, and cultural offerings, all connected to local and regional transit.

Delivering a mix of flexible office space and potential educational uses, with a focus on innovative industries, this district will ensure that Lakeview Village is an animated, populated space throughout the day and evening – supporting the retail and cultural uses with its captive market and creating new synergies for collaboration.



Figure 5.2.4a - Flexible office space



Figure 5.2.4b - Educational facilities and campus environment



Figure 5.2.4c - Location of Serson Innovation Corridor



Figure 5.2.4d - Integrated sustainable design



Figure 5.2.4e - Diversified functions



Figure 5.2.4f - Innovative industries and institutions

5.2.5 THE MARINA

Offering lakefront living across from Mississauga’s public marina with panoramic views of the lake, this neighbourhood will be defined by its iconic buildings, setback from the public park, and trail systems continued from Waterway Common and Inspiration Point.

Having ‘earned the sky’ through world-class architecture and design, the Marina district will offer unparalleled views of Lake Ontario while adding to the City’s skyline as a new showpiece. As a trailhead to the region, the public parks that front and weave into the centre of this neighbourhood will offer residents and visitors access to the lake’s edge and a connection to the region’s Waterfront Trail. The Marina will also be home to two unique destination restaurants that will act as anchors to the neighborhood, animating the park, and ensuring that views and the waterfront experience are available to all.



Figure 5.2.5a - Lakefront living and iconic buildings adding to the City’s skyline

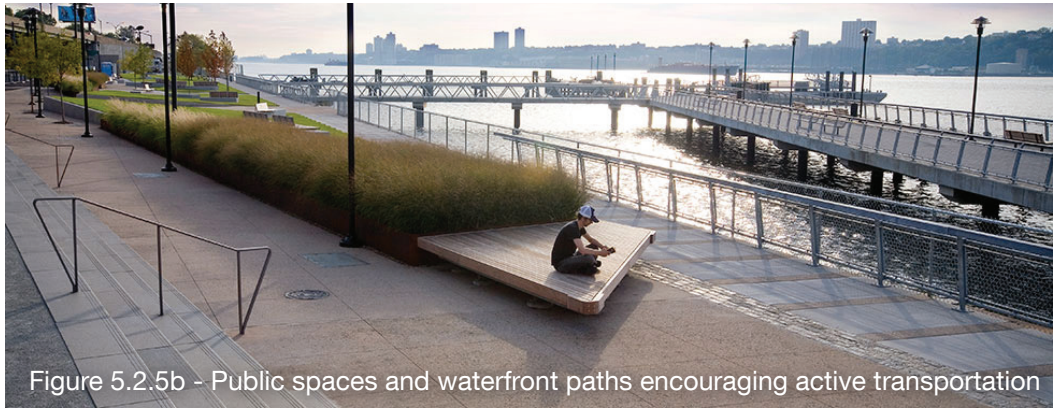


Figure 5.2.5b - Public spaces and waterfront paths encouraging active transportation

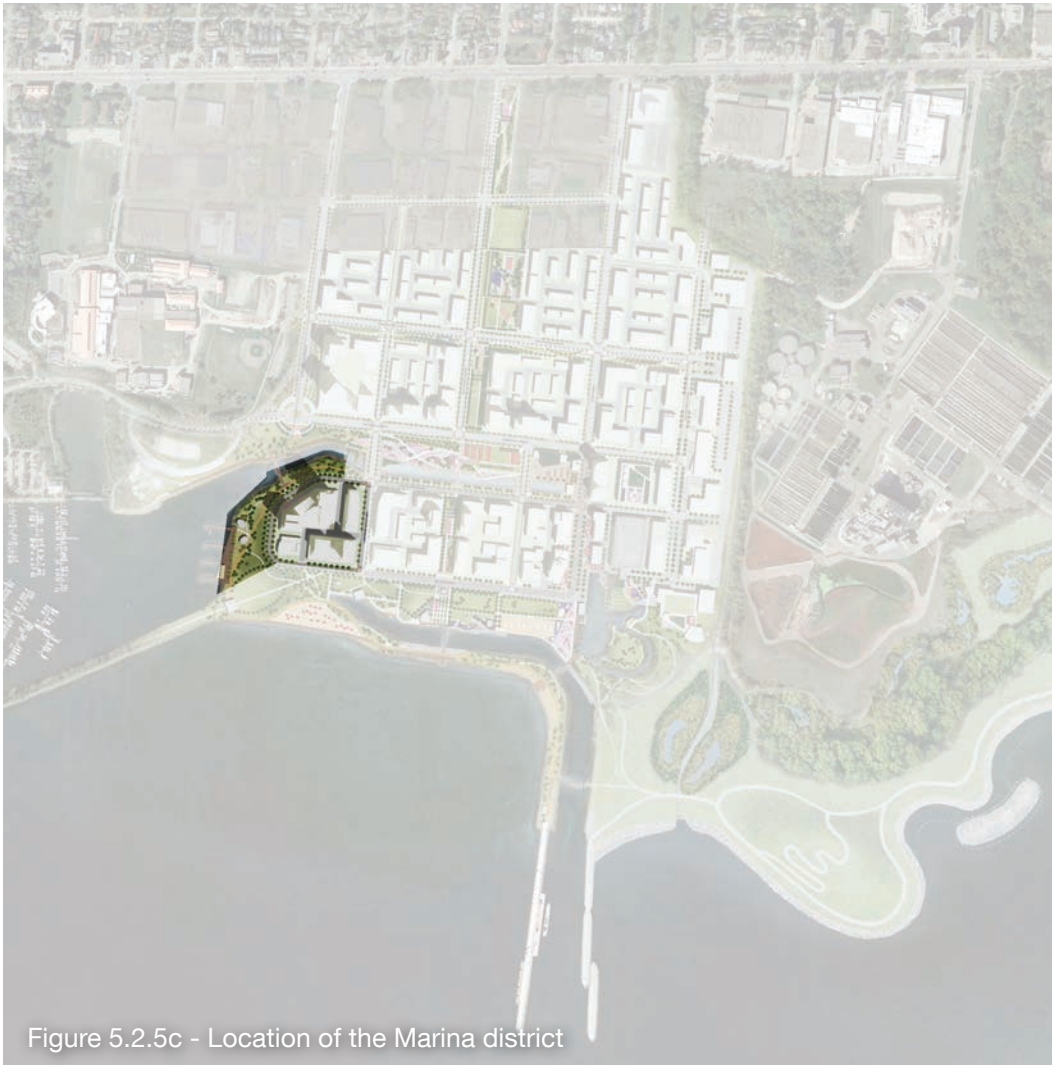


Figure 5.2.5c - Location of the Marina district

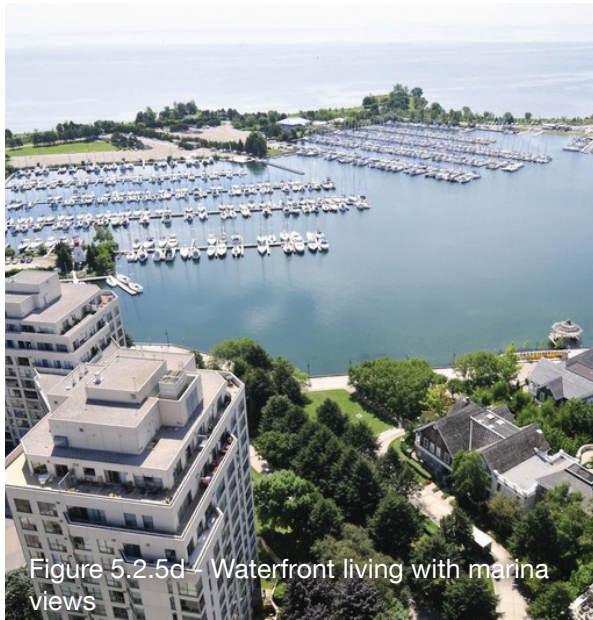


Figure 5.2.5d - Waterfront living with marina views



Figure 5.2.5e - Public dock facilities unique to Lakeview Village



Figure 5.2.5f - Waterfront activities for all ages

5.2.6 OGDEN GREEN

Predominantly residential in nature, Ogden Green will include a mix of townhomes and mid-rise dwellings surrounding a neighbourhood park.

Ogden Green will be designed to focus on community, with the park providing a central gathering space. Woven into the wider Village through numerous trail connections, pedestrian-scaled streets will be well-connected with Waterway Common, the waterfront, and Lakeview Square. The central location of this neighbourhood offers residents the opportunity to take full advantage of community living in Lakeview Village.

Within the Ogden Green district, integrated green spaces tie the neighbourhoods together, which may include features such as courtyards, mews, and green roof decks.



5.2.7 LAKESHORE GATEWAY

The signature entry into Lakeview Village, Lakeshore Gateway will be a signal to the region of the unique experiences offered at the Village.

Defined by a mixed-use landmark building on Lakeshore Road, designed to be a signature of world class architectural standard, this gateway district will offer daily amenities such as a grocery store, pharmacy, bank, and other goods-and-services. Lakeshore Gateway will provide an additional mix of second-floor residential and office uses alongside the Serson Creek corridor, further weaving the innovation campus into the wider community and creating potential new housing partnerships and employment options with campus users.



Figure 5.2.7a - Strong built form massing as a gateway into Lakeview Village

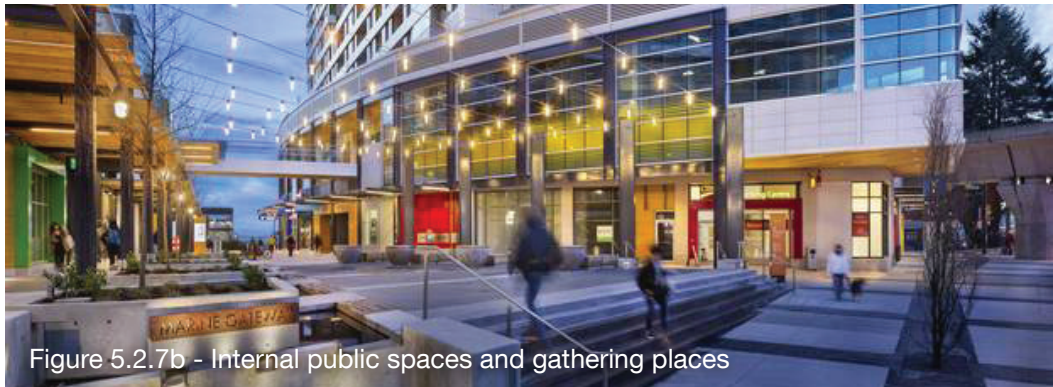


Figure 5.2.7b - Internal public spaces and gathering places

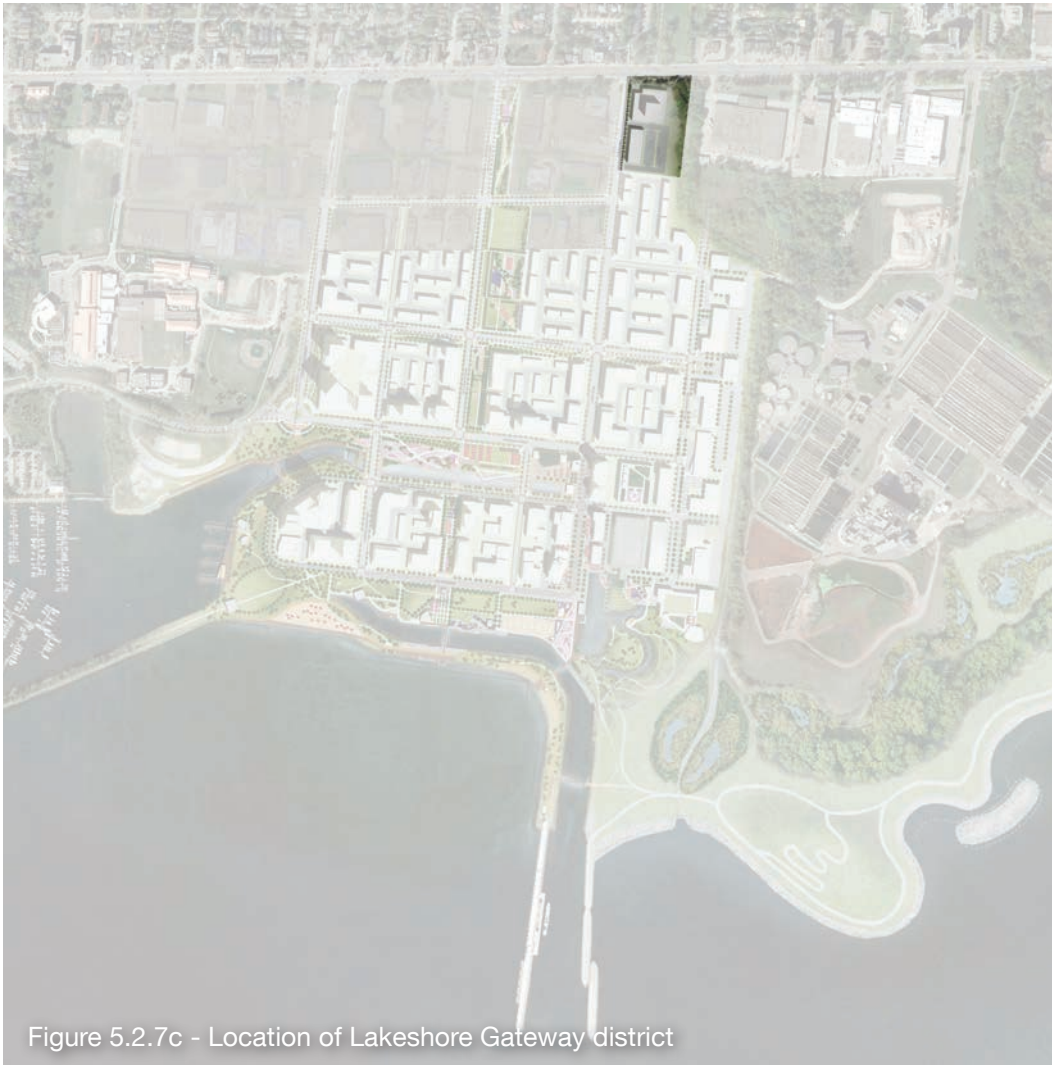


Figure 5.2.7c - Location of Lakeshore Gateway district



Figure 5.2.7d - Potential mix of uses including employment, commercial, and residential



Figure 5.2.7f - Achieve an appropriately scaled transition from gateway to residential

5.2.8 FUTURE DISTRICTS

Future districts are parcels of land that are not owned by LCPL but are included in the Lakeview Major Node Character Area.

These parcels are subject to the City's MOP policies, including issues surrounding municipal comprehensive review requirements, and have the potential to develop over a longer period of time, as they contain existing businesses, and development would require the sale and land assembly of various parcels.

These future districts along Lakeshore Road and adjacent to Ogden Green district will be well-positioned to respond to market demand and continue to provide a range of residential, commercial, and retail offerings to the community. The eventual development of the future districts will ensure the complete extension of Ogden Green park, a key community spine connecting to the water's edge.

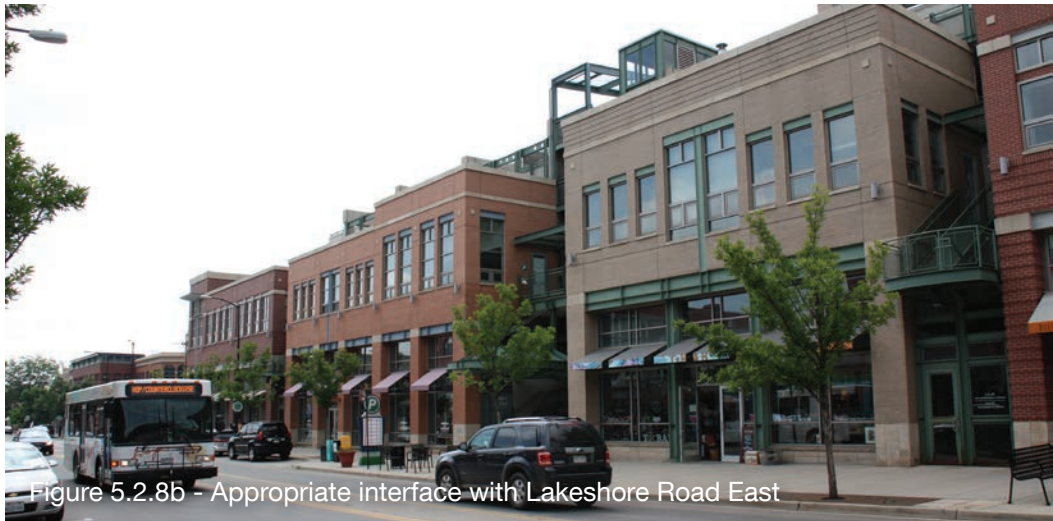




Figure 5.3a - Multi-use path and active waterfront public space

5.3 WATERFRONT, PARKS & OPEN SPACES

The Lakeview Village DMP includes a variety of public parks and open spaces, including active waterfront lands, a recreational pond, a vibrant public square, quiet green spaces, and Waterway Common, a unique, multi-use passive and active park with four season programming and activity.

5.3.1 OPEN SPACE CONNECTIVITY

Lakeview Village will be a healthy community with pedestrian friendly streets and neighbourhoods, amenities within walking distance, an active lifestyle encouraged through bike lanes, trails, parks, waterfront facilities, as well as a detailed retail program and associated cultural amenities.

LAKEVIEW SHORELINE

The extensive shoreline at Lakeview Village will consist of multi-use paths, passive and active amenities, public spaces, and gathering places. The shoreline and its multiple uses will enhance access to the waterfront and increase connections to adjacent parks, a conservation area, and the Waterfront Trail.

LAND CONVEYANCE

The Lakeview Village master plan has conveyed 67.1 acres (27.15 ha.) of land to the City of Mississauga for parks, open space, and cultural uses. The conveyed lands encompass 16.56 acres (6.70 ha.) of greenlands and 17.40 acres (7.04 ha.) of public open space including all of Inspiration Point (Panorama Point Park, Channelside Park, Western Pier, and Inspiration Park). Other designated uses within the conveyed lands include 9.02 acres (3.65 ha.) of Institutional lands and 2.84 acres (1.15 ha.) for mixed-use cultural hub uses in Waterway Common.

JIM TOVEY LAKEVIEW CONSERVATION AREA

The Jim Tovey Lakeview Conservation Area, a new 64 acre (26 ha.) conservation area south-east of the site, is currently under construction. The conservation area is a joint effort of the Region of Peel, Credit Valley Conservation (CVC), and Toronto and Region Conservation Authority (TRCA) to establish a publicly-accessible protected green space where there is currently no public access to the lake. Located to the immediate south-east of the site, adjacent to Inspiration Point, the new lands will connect Lakefront Promenade Park, Marie Curtis Park, and Lakeview Village, providing the direct missing link of the waterfront trail along Mississauga’s shoreline. The new green space will reclaim wildlife and fish habitats, create new coastal meadows, and tiered wetlands, becoming an unprecedented amenity for Lakeview Village and the Mississauga community.





Figure 5.3b - Preliminary Lakeview Village Master Plan



Figure 5.3c - Ogden Green Park



Figure 5.3d - Waterway Common

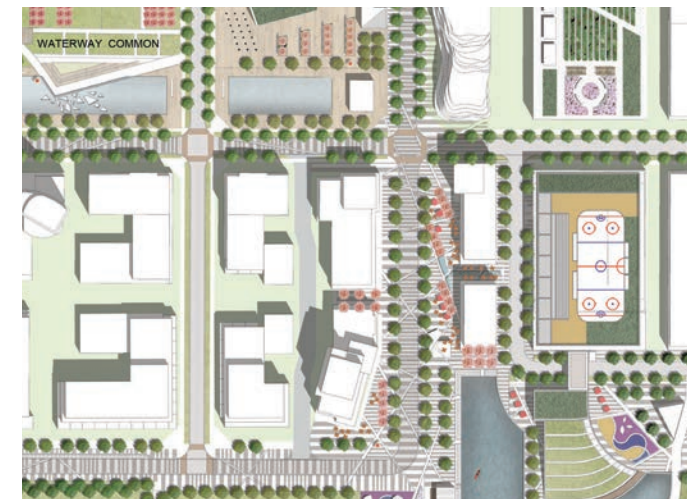


Figure 5.3e - Lakeview Square



Figure 5.3f - Recreation Pond

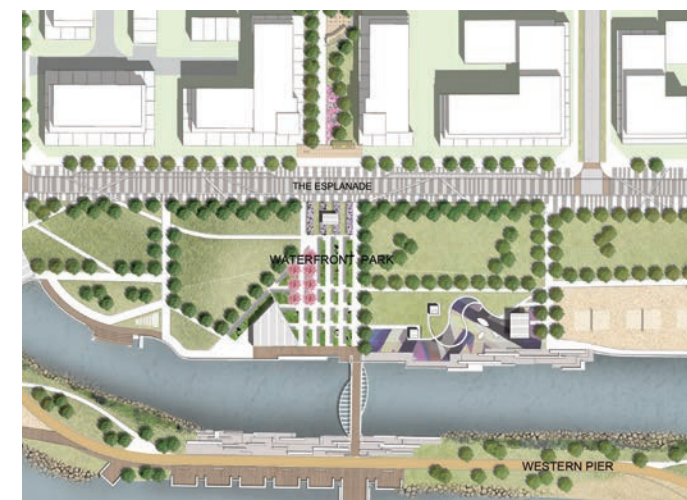


Figure 5.3g - Waterfront Park



Figure 5.3h - Western Pier



5.3.2 SERSON CREEK

Restoring a naturalized creek with native vegetation will transform the urban environment, enhance and replenish coastal habitats, and encourage public stewardship along Serson Creek and the waterfront.

Serson Creek is a narrow watercourse that extends from north of Lakeshore Road East and runs along the eastern edge of the site, adjacent to the G.E. Booth Wastewater Treatment Facility. Following years of environmental deterioration from near-shore stone mining and utility use, the mouth of Serson Creek is part of a larger restoration project within the Jim Tovey Lakeview Conservation Area. In 2018, The Jim Tovey Lakeview Conservation Area project will re-establish a new river channel to connect Serson Creek to Lake Ontario. With this portion of the creek formerly buried in a pipe underground, the



Figure 5.3.2a - Strategic enhancements can enable a naturalized creek to thrive in an urban setting.

modification will allow fish to access the creek for the first time in decades. It is anticipated that the Serson Creek wetland to the south-east of Lakeview Village will provide valuable fish habitat and wildlife viewing opportunities for visitors.

The restoration of Serson Creek within Lakeview Village's lands includes a portion that is situated along Serson Innovation Corridor. The proximity of the creek to future educational / institutional facilities provides an opportunity for immediate connections to wildlife, while supporting learning and exploring of local biodiverse habitats.



Figure 5.3.2b - Photo of the existing state of Serson Creek within the Lakeview Village lands.



Figure 5.3.2c - Naturalized creek adjacent to open space and trail

5.3.3 PARK TYPOLOGIES & PROGRAMMING

An interconnected system of parks and open spaces has been designed to provide a range of passive and active recreation opportunities within walking distance of all districts and neighbourhoods that contributes to the character and identity of Lakeview Village and reinforces a healthy, active, and vibrant community.

Parks proposed for the community shall consist of five types, all of which are defined through function, configuration, setting, and programming opportunities. These include the following:

- **Waterfront** - Panorama Point Park, Channelside Park, Inspiration Park, and Western Pier;
- **Waterway Common;**
- **Neighbourhood Park** - Ogden Green and Ogden Vista Park;
- **Linkage Park** - Aviator Greenway, Lakefront Greenway, and Serson Greenway;
- **Lakeview Square.**

Combined, these parks will form a continuous open space linkage system that will connect all districts and neighbourhoods internally within Lakeview Village and externally with existing park systems to the east and west, including the emerging Jim Tovey Lakeview Conservation Area to the east.

Lakeview Village’s extensive parks and open space system provides opportunities for potential collaboration with local and citizen-led initiatives that foster native planting projects, such as The David Suzuki Foundation’s *The Butterflyway Project*. This initiative has successfully collaborated with schools, city agencies, and homeowners across the GTA to create neighbourhood-scale wildflower corridors. With native wildflower gardens planned for the major parks, including the Waterfront, Ogden Green and Waterway Common, providing pollinator patches for bees and butterflies will help establish corridors of natural habitat in Lakeview Village's districts.

The following section describes the character and intent of these park types and prescribes potential programming opportunities that will emphasize a unique and innovative approach to public space and place-making in the City of Mississauga.



Figure 5.3.3a - Parks as community gathering spaces



Figure 5.3.3b - Linking the waterfront trail



Figure 5.3.3c - Innovative park design and programming



Figure 5.3.3d - Variety of waterfront interface conditions





Figure 5.3.3f - Publicly accessible waterfront amenity with seating areas

A. WATERFRONT

The waterfront park system will comprise of four identified park areas, including:

- **Panorama Point Park** - situated to the west and includes the existing pier;
- **Channelside Park** - centrally located along the waterfront, framed by residential built form along the north side and the channel along the south;
- **Inspiration Park** - situated to the east with direct connections to Lakeview Square, Serson Innovation Corridor, and the conservation lands;
- **Western Pier** - east-west component forms the south shoreline edge and swings southward to provide a dramatic view opportunity.

These four park areas will combine to form the waterfront, a seamless lakeside destination that provides a dramatic setting and major draw for the Lakeview community and the broader region. As such, proposed features, facilities, and programming are described collectively as the waterfront. The following are some of the key characteristics:

- The new publicly accessible waterfront will provide the opportunity to link the Waterfront Trail along the water's edge, utilizing the Western Pier to ensure continuous shoreline exposure from the existing Lakefront Promenade Park in the west through the emerging conservation area and existing Marie Curtis Park to the east;

- A variety of edge conditions along the interface with the channel and the lake will balance functional and programming requirements to create a series of compelling water engagement opportunities;
- A great lawn in combination with a potential extension of a trail along the existing breakwater pier (pending CVC and Department of Fisheries and Oceans (DFO) confirmation) along the western edge will provide panoramic views of Lake Ontario and the adjacent Lakefront Promenade Park;
- Publicly accessible docking facilities for small craft boats conducive to the expected limited water clearance will activate the west shoreline adjacent to The Marina district and provide a continuation of the nearby uses represented by the publicly-owned Lakefront

Promenade Public Marina and the private Port Credit Yacht Club;

- An urban beach along the southern shoreline with a boardwalk edge, distinctive seating features and nearby changeroom/washroom kiosk;
- An accessible pebble beach opportunity adjacent to the bend of the western pier that will enable people to play on the shore and step into the water within a natural setting;
- A paddle sport launch and rental kiosk will enable participants to gain access to the channel and lake with kayaks, canoes and standup paddleboards, reinforcing the primary principle of allowing people to engage the water in pursuit of active recreation;



Figure 5.3.3g - Conceptual recreation pond well linked with Lakeview Square, the waterfront and surrounding amenities



Figure 5.3.3h - Conceptual western pier with areas for viewing and seasonal amenity functions



Figure 5.3.3i - Variety of seating areas, gathering areas, and viewing opportunities along the waterfront



Figure 5.3.3j - Multitude of water's edge interface conditions (urban beach)



Figure 5.3.3k - Variety of active recreation opportunities on land



Figure 5.3.3l - Variety of active recreation opportunities in the water

- A combination of active and passive open space areas that will allow for informal play, as well as provide areas for naturalized landscaping;
- Opportunity to integrate a series of unique pedestrian bridges that will help define the character of the waterfront and facilitate channel crossings and trail system linkages;
- A variety of seating areas and lookout opportunities that will integrate a distinct palette of park furniture and lighting;
- A plaza space at the water's edge terminus of Ogden Vista Park that will provide flexible use, community gathering space for day-to-day waterfront use, and for special events to occur. It will be designed as an attractive, functional space with ample seating and shade elements that will reinforce user comfort;
- Pop-up or seasonal food/beverage kiosk or venue adjacent to the plaza space with water's edge views and opportunities for patio seating;
- Children's play zone that will integrate water play opportunities and other elements unique to the waterfront;
- Beach volleyball venue that provides additional active recreation opportunities and team based community engagement. This will be supported by changeroom / washroom kiosks that can be shared with the children's play zone;
- The variety of kiosks are an opportunity to reflect a unique design emphasis, material palette, and architectural style that can help define the waterfront's character;
- A recreation pond with integrated paddle sport launch and rental kiosk facilities is proposed along the east edge of the waterfront to provide protected waters for novice paddlers that may be intimidated by the channel waters and beyond. This is intended to reinforce accessibility to water activities for a wide user base. An additional paddle sport launch is provided towards the west end of the channel, catering to those who may want to connect with the lake.
- The Coal Hill music amphitheatre venue will merge entertainment programming with dramatic views of the lake, attracting local and regional visitors to the waterfront and surrounding districts and amenities;
- The Western Pier will provide the most dramatic and long-range views. Stretching out more than 600 metres into Lake Ontario, the pier affords panoramic views seemingly within the middle of the water that is typically exclusive to boaters. To activate the pier and encourage use, several elements will be placed along the pier, including seating and lookout features, windbreaks and warming stations. There is the opportunity to accommodate large boat docking along the pier to encourage visitors and allow for interesting events such as a tall ships festival. Additionally, the former ship loading functions provide an opportunity for adaptive re-use of crane infrastructure as a reflection of the site's industrial past.



Figure 5.3.3m - Preliminary concept of Waterway Common



B. WATERWAY COMMON

The integration of Waterway Common park as a central focus for the community provides the opportunity to complement waterfront activities with a major interior landscape space that is strategically linked with Lakeview Square and Ogden Green to support a range of uses and programming. In doing so, Waterway Common will serve to physically and visually link the existing park system to the west with the retail and cultural centre of Lakeview Square.

Waterway Common will contain several elements that define its programming and function from a passive and active use standpoint. These elements will help reinforce one of the main themes of this primary public open space, which is to reflect the transition from a more structured urban environment at the east end interface with Lakeview Square, to a more naturalized landscape treatment as it leads towards the existing outlet and parks to the west.



Figure 5.3.3n - Conceptual plan of Waterway Common



Figure 5.3.3o - Integrated stormwater management functions

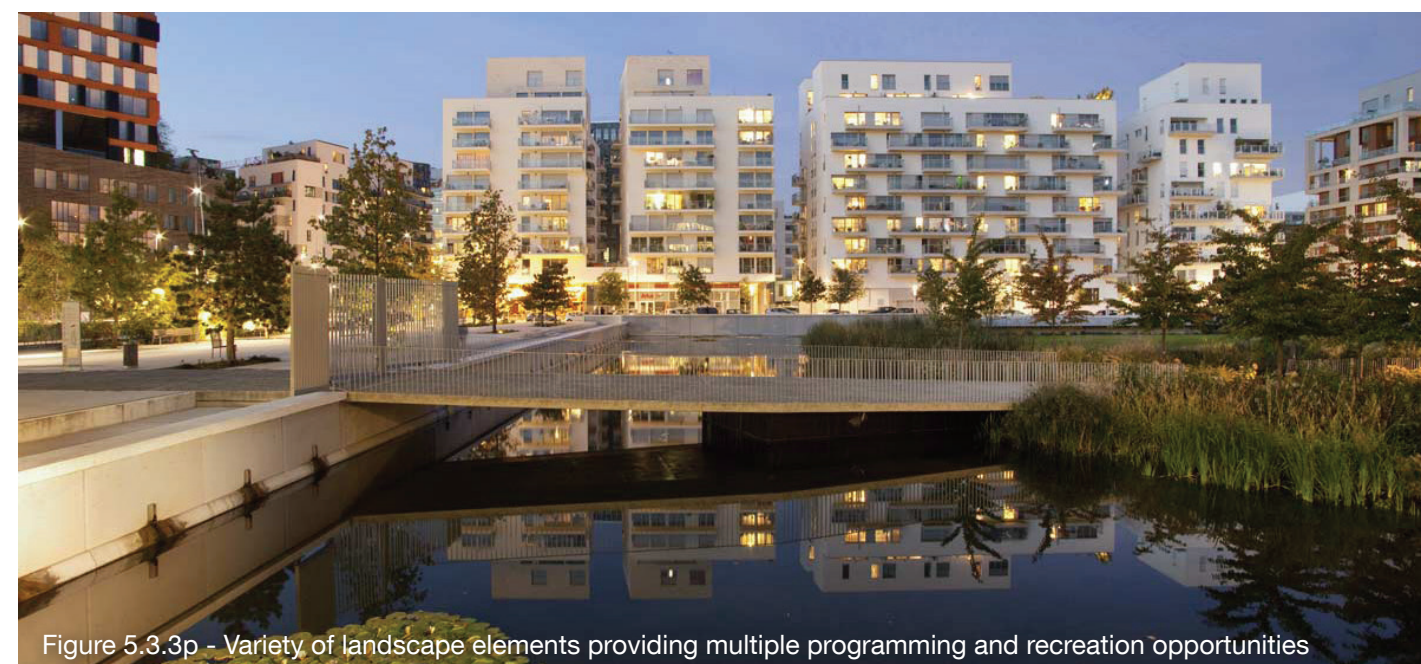


Figure 5.3.3p - Variety of landscape elements providing multiple programming and recreation opportunities



Figure 5.3.3q - Preliminary concept of Waterway Common's four season outdoor space

The following elements, programming ideas, and characteristics are currently being considered:

- A canal feature that runs along the south side of the Common for the full extent of the park, providing a variety of unique water play, planting, and edge conditions that will animate the space and integrate the adjoining street (Serson Promenade and Waterway Street) and built form uses;
- Due to grading constraints, issues related to fluctuating water levels, edge conditions, and water quality maintenance, the canal is intended as a reflecting pool with a circulating closed water system, rather than a direct extension of the existing outlet to the west. However, the intended perception is that the canal will feed into the outlet and, in doing so, will become linked with the lake;
- The canal may provide opportunities for changing water levels (weirs and drops) that can relate to variations in how the adjacent street and park elements integrate along the edges;
- A unique pedestrian bridge will link the north and south side of the canal in alignment with the Ogden Avenue view corridor, reinforcing a direct connection with the lake;
- The east section of the canal, adjacent to Lakeview Square, will become an ice rink in the winter, providing year-round programming and activities to support the Square as the community retail and cultural hub;
- The canal provides an opportunity to integrate stormwater treatment functions as a component of a comprehensive and innovative sustainability strategy;
- Park users will be encouraged to touch, listen, and engage with the water in multiple ways that will reflect the broader principle of attracting people to the space for a variety of active and passive activities and events;
- A significant public art installation that will see the original 'Four Sisters' smokestacks represented in a creative and prominent vertical art expression, emblematic of the beacon function the stacks played for seafarers looking to the shoreline;
- A cultural or community use building will be provided adjacent to Lakeview Square, integrating innovative sustainability practices (green roofs, water harvesting, energy efficiency) with high quality architectural design that helps frame open space features within the Common and the Square. These buildings will bring year-round programming opportunities to Lakeview Village that will attract local and regional visitors;
- An extensive network of pathways will navigate throughout Waterway Common, providing critical pedestrian linkages to the water's edge at the extension of Ogden Avenue and with the existing park system to the west, as well as neighbourhoods and open spaces to the north (Ogden Green park);
- The network of pathways will change in level in response to viewing opportunities and the integration of lookout terraces, small scale performance stage, a dramatic event pavilion and other uses where transitions in grade will help elevate the impact of these elements;



Figure 5.3.3r - Year-round activation and interest



Figure 5.3.3s - Public cultural use facilities integrated with the park

- Planting within Waterway Common will help to form and characterize various ‘outdoor rooms’, and is intended to include a celebration grove of flowering trees, native fragrant and wildflower gardens, aquatic planting as a transition to the outlet and park system beyond, as well as significant shade tree planting to maximize canopy coverage;
- The combined planting and network of pathways will be used to frame the open spaces within the park that will function as active and passive greens (picnicking, frisbee play, kite flying etc.);
- Streets framing the north and south sides of Waterway Common (Waterway Street and Serson Promenade) will be designed with a scale and streetscape that reinforces a strong connection between Waterway Common, the streets, and the adjacent built form. These streets will reflect a pedestrian priority through appropriate scale, materiality, lighting, and furniture palette that will achieve a street that feels like an extension of the park while balancing vehicular requirements, including transit;
- The strong built form edge along the north and south sides, consisting primarily of mid-rise residential buildings, will be of a scale that comfortably encloses and frames Waterway Common.



Figure 5.3.3t - Providing passive and active recreation uses



Figure 5.3.3u - Fragrant and wildflower gardens



Figure 5.3.3v - Ornamental flowering trees



Figure 5.3.3w - Festival pavilion for music events and gatherings



C. LAKEVIEW SQUARE

Lakeview Square will be a vibrant central gathering place and public plaza with four-season programming that will serve as the retail, arts and cultural hub for Lakeview Village.

Drawing from the community and beyond, the Square will function as a flexible urban open space that can be programmed to accommodate a variety of events and celebrations. Public markets, art fairs and festivals will occur, all supported by the curated retail, restaurant and services integrated into the ground floor of adjacent mixed-use buildings.

The combination of appropriately scaled buildings with animated storefronts on the east side and a ‘woonerf’ inspired shared street will frame the space and provide a safe, comfortable and inviting pedestrian focused environment. Decorative paving within the Square will extend across the adjacent street to provide a sense of entry, reinforce the pedestrian priority for the



Figure 5.3.3x - Formal gathering space with flexible programming

combined space and serve a traffic calming element. It also provides the potential to expand programming space on a temporary basis.

Other landscape elements and uses integrated with the Square include unique seating and lighting elements (including moveable chairs and tables), a water feature, interactive information kiosks, stormwater planters, public art, performance stage and seasonal vendors.

The Square will be seamlessly linked with the Waterway Common to the west and the Waterfront to the south to provide a continuous public open space system that delivers a variety of experiences, activities, programming opportunities and settings that will define the character of Lakeview Village.



Figure 5.3.3y - Distinctive paving and patterns extending through the landscape



Figure 5.3.3z - Conceptual Plan of Lakeview Square



Figure 5.3.3aa - Flexible space for a variety of programming



Figure 5.3.3bb - Unique landscape features



Figure 5.3.3cc - Plaza paving will extend across the adjacent street to reinforce the sense of place



Figure 5.3.3dd - Pedestrian-oriented public plaza with landscaping and seating features

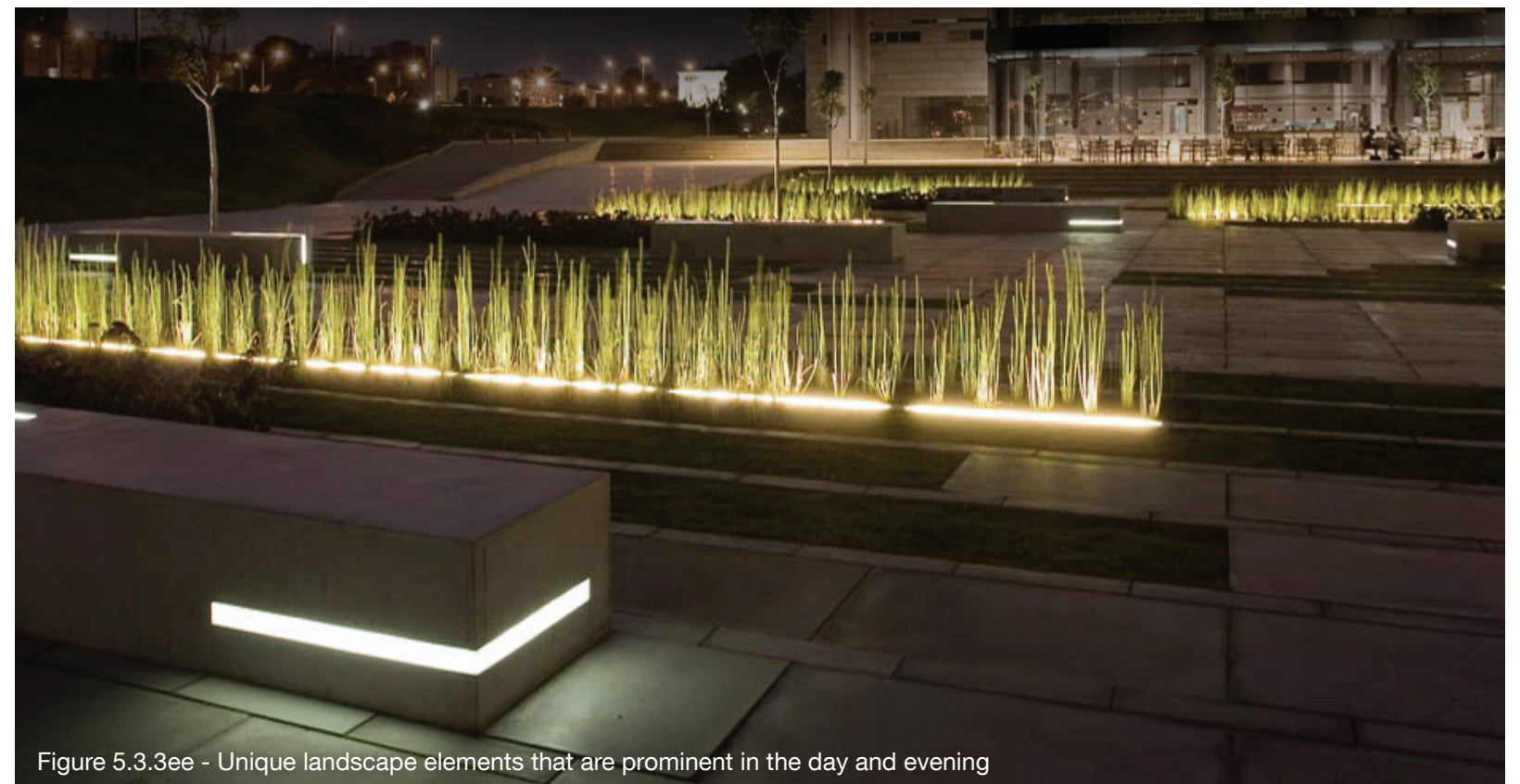
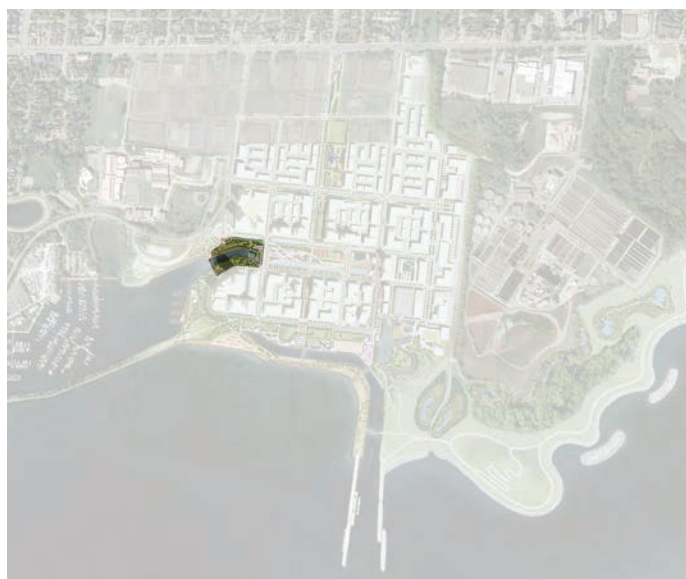


Figure 5.3.3ee - Unique landscape elements that are prominent in the day and evening



D. LAKEFRONT PROMENADE PARK

Lakefront Promenade Park will function as passive use open space and link that extends from the existing park system to the west and connects it with Waterway Common as a key part of a comprehensive system of park links for Lakeview Village.

The park extension will contain the Waterfront Trail and allow it to extend down and across the water's edge to achieve the continuous waterfront link for this area of the Mississauga shoreline. It may include a significant public art feature as part of a series of installations throughout Lakeview Village that commemorates the beacon function of the original smokestacks that lined the shore, or potentially the lands aviation and military history. The location of this feature within the park will also align with the view corridor along Lakefront Promenade, creating in essence a pivot point for linking the major community gathering spaces to the east and south.



Figure 5.3.3ff - Linking the new park with the existing park along the water's edge

The south edge of the park will front onto the north side of the lake outlet, extending the shoreline and allowing the existing boardwalk to be extended to the outlet headwall.

The canal pool within Waterway Common will appear to flow into the outlet, providing the sense of a continuous flowing waterway link and reinforcing a seamless connection between the two park areas.

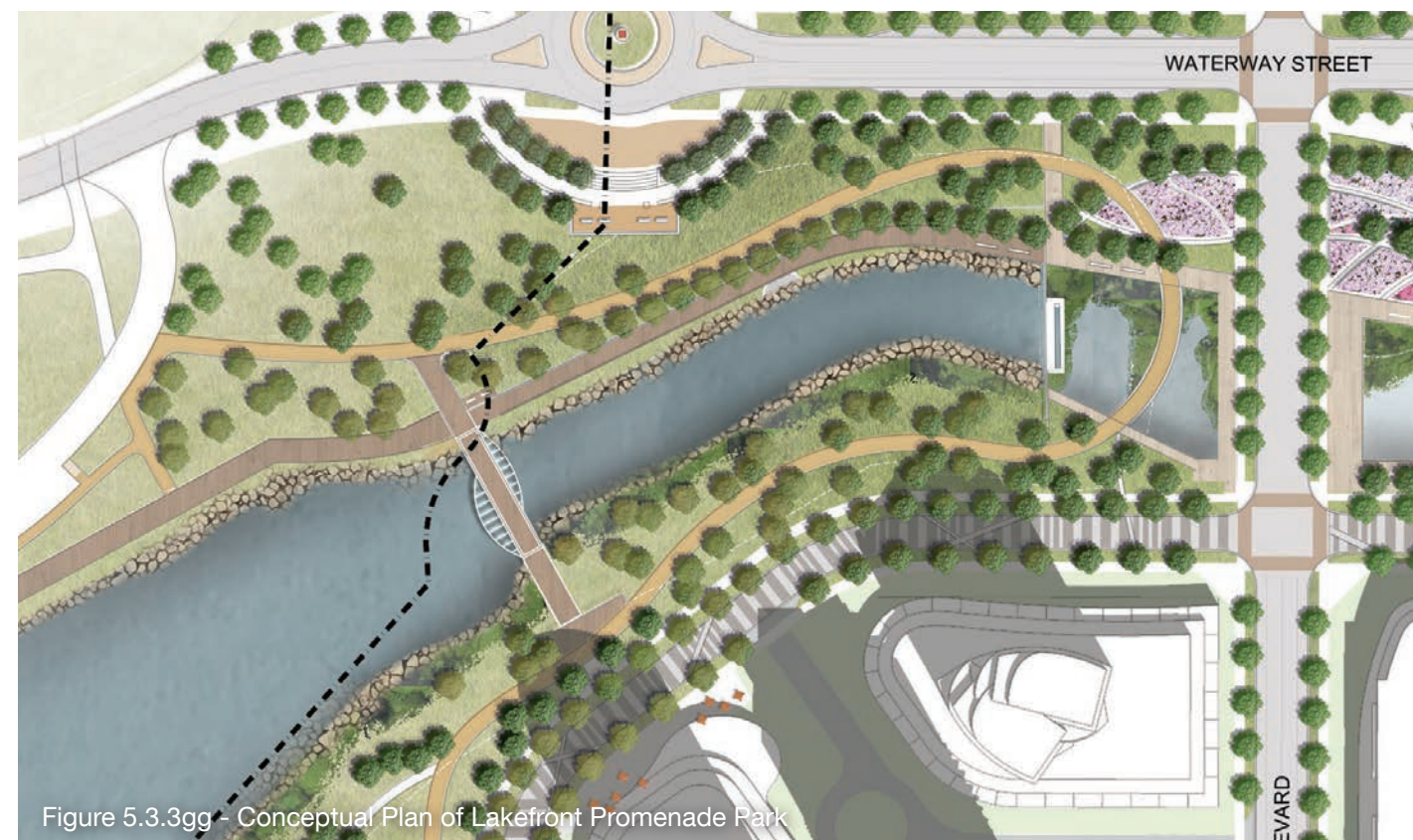


Figure 5.3.3gg - Conceptual Plan of Lakefront Promenade Park



Figure 5.3.3hh - The existing boardwalk along the channel outlet will be extended into the community and Waterway Common



Figure 5.3.3ii - Multi-Use Trail along the water's edge



Figure 5.3.3jj - Park amenity features situated along the waters edge



Figure 5.3.3kk - A pedestrian bridge will extend across the channel and serve to connect the waterfront trail



Figure 5.3.3ll - Potential for a public art feature



E. OGDEN GREEN

Ogden Green park is intended as the central focus and primary green space for Ogden Green district.

Stretching north-south over three (3) blocks adjacent to New Ogden Avenue, the linear park provides a continuous view corridor and is primarily surrounded by a mix of low to mid-rise residential dwellings. The central park of Ogden Green (approximately 1.7 ha. / 4.1 ac.) will provide a variety of passive and active recreation elements and facilities that will serve the surrounding residents. These features will follow a more standard park programming strategy and will complement the nearby Waterway Common park, as well as connecting linear park systems, including Aviator Greenway, which runs along the south side of New Aviator Avenue and connects Ogden Green with the linear park systems to the east and west.

The following are some of the key attributes proposed for this district focused park:

- Ogden Green will be designed as the central focus of the surrounding residential neighbourhood;
- The park will be sited with frontages on New Ogden Avenue to promote views and access;
- Ogden Green will be predominantly soft landscaped to allow for a variety of active and passive uses, including programmed and unstructured uses;
- Ogden Green is intended to feature a variety of active and passive features, including an athletic field, hardcourt play surface, children's play structure and water play, a dog park, community gardens, extensive seating areas and public art;
- A variety of planting approaches will be seen, from naturalized native tree, grass and shrub planting to formal groves of flowering trees serving to reflect elements of Waterway Common to the south;
- The park will be well-linked through a series of paths that will support active transportation and provide continuous links with the Greenway park system and Lakeview Village park network;
- Planting (trees, shrubs, grasses) will consist of species tolerant of urban conditions with an emphasis on native species;
- Bike racks will be installed in Ogden Green and all other parks as part of the outdoor furniture program to promote cycling connections throughout Lakeview Village;

- Key features of the park will be sited to align with view corridors. The design of hard and soft landscape elements and features, including points of entry, will be consistent and / or complementary with established neighbourhood themes (including surrounding dwellings and other open space components).



Figure 5.3.3mm - A space for dog walking



Figure 5.3.3nn - Interactive water play facilities



Figure 5.3.3oo - Conceptual Plan of Ogden Green Park



Figure 5.3.3pp - A series of active and passive use spaces with walkways, seating, and play elements



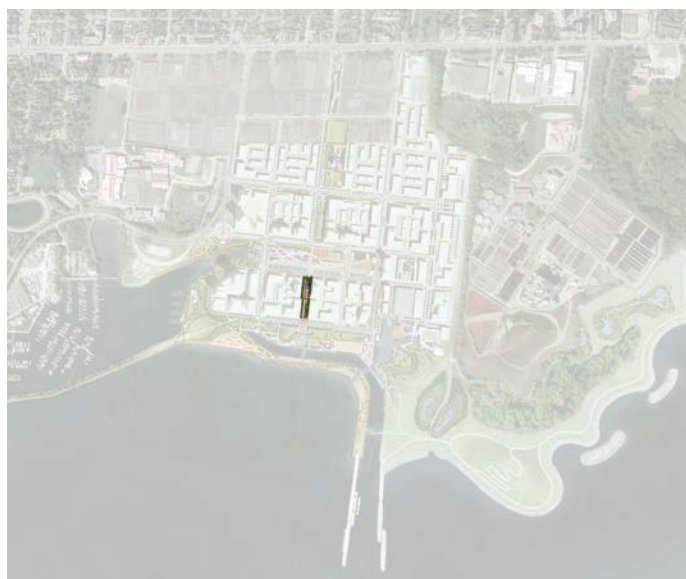
Figure 5.3.3qq - Park designed as the central focus of the surrounding residential neighbourhood



Figure 5.3.3rr - Combination of hard and soft landscape features to define spaces and uses



Figure 5.3.3ss - Play facilities as a major element of the park



F. OGDEN VISTA PARK

Ogden Vista Park is a centrally located open space feature designed to bring people closer to the waterfront.

Extending south from Waterway Common, Ogden Vista Park will align with the future New Ogden Avenue to create an extended view corridor from Lakeshore Road East down to the water. It will provide a direct link for people to walk and cycle from Ogden Green and Waterway Common to Inspiration Point and waterfront activities, reinforcing the key principle of bringing the people to the water.

Ogden Vista Park will be characterized by a formal landscape structure comprising defined groupings of plantings, walkways and a central seating and gathering space. It will emphasize direct physical and visual links with the waterfront and lake aligned with the extension of the Ogden Avenue view corridor and north-south park system. The park will, therefore, be designed to safely and conveniently accommodate walking



Figure 5.3.3tt - Extended view corridor down to the water

and cycling connections, while creating spaces comfortable for seating and small gatherings. Consistent with parks proposed throughout Lakeview Village, public art can be integrated as a focal element that helps tell the story of Lakeview.

The park edges will be defined by a combination of mid and higher storey residential buildings, which will have a strong architectural orientation to the park space to achieve a comfortably scaled 'outdoor room' and help frame views to the lake. The massing of the buildings will consider wind, sun and shadow impacts to ensure a comfortable environment within the park is achieved to maximize pedestrian use throughout the year.

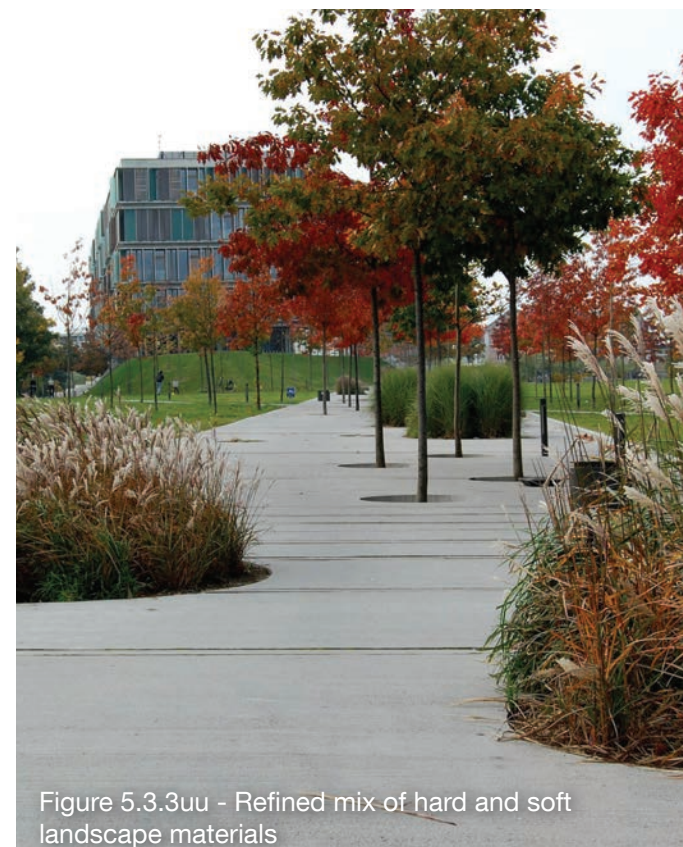


Figure 5.3.3uu - Refined mix of hard and soft landscape materials



Figure 5.3.3vv - Unique architecture framing an important pedestrian corridor

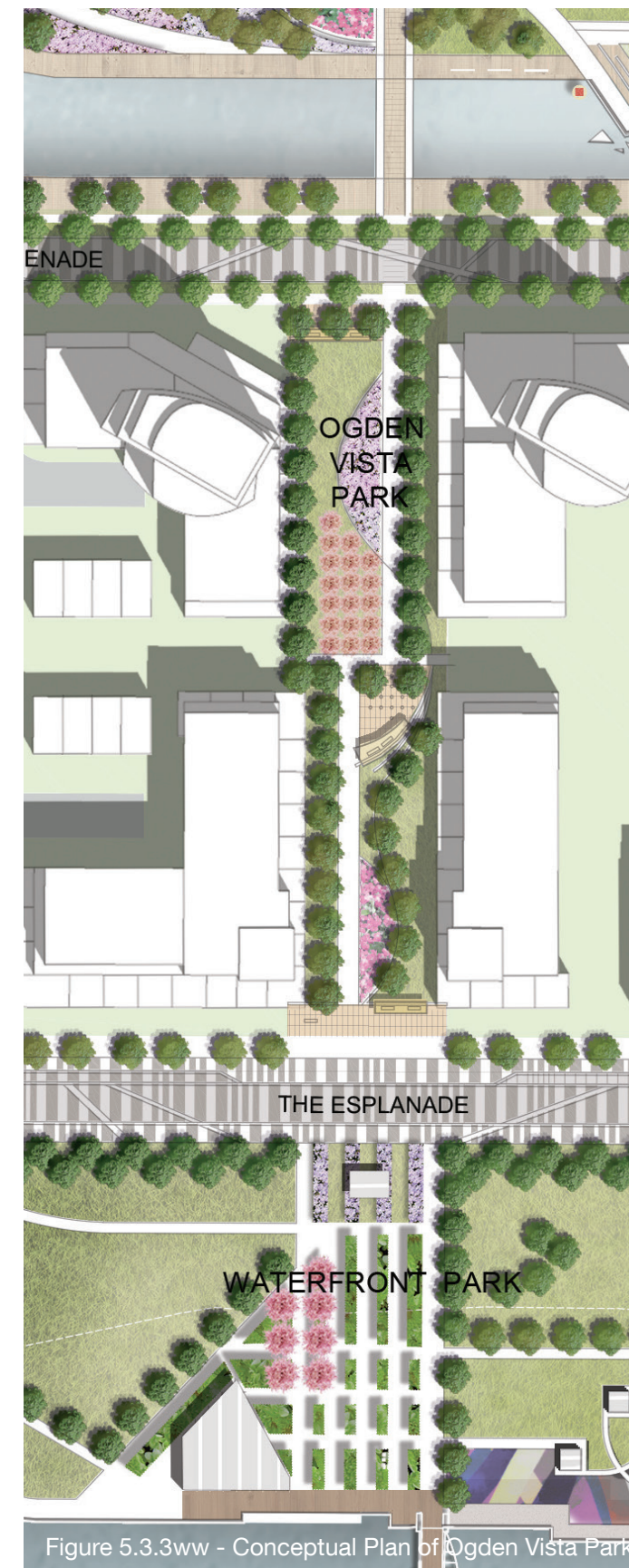


Figure 5.3.3ww - Conceptual Plan of Ogden Vista Park



G. AVIATOR GREENWAY & LAKEFRONT GREENWAY

Aviator Greenway and Lakefront Greenway are strategically located linear parks that will establish the continuously connected park and open space network for Lakeview Village and beyond.

At an approximate width of 15 metres, Aviator Greenway will run along the south side of New Aviator Avenue from New Haig Boulevard to Lakefront Promenade, where it will then transition as Lakefront Greenway and run south to Waterway Street. The alignment enables these linear parks to be directly connected with Ogden Green Park, Lakefront Promenade Park and the existing and proposed park network further extended from these open spaces.



Figure 5.3.3xx - Conceptual Plan of Aviator Greenway

Although intended primarily as a multi-use trail link for active transportation options (walking, running, cycling, rollerblading, etc.), the Greenways will integrate seating and shade structure amenities at key areas and points of entry.

Vegetation will be dominated by the planting of large crown shade trees situated in clusters to achieve a pleasant and desirable park experience. The Greenways may also feature a 'fit-trail' fitness system with stations strategically located along the linear park to provide additional opportunities for exercise and reinforce a healthy lifestyle approach for the community.

Built form will have a strong relationship with the Greenways, with appropriate architectural elevations oriented to the park to reflect the visual prominence of the buildings in forming the character of the public realm. In many instances, direct 'front-door' links from building entrance to park trail may be provided to better engage architecture and open space.

The Greenways are a critical component in establishing a fully connected park and open space network that will link all neighbourhoods with the water.





I. SERSON GREENWAY

Serson Greenway is a natural corridor that encompasses the extension of Serson Creek from Lakeshore Road east south to the emerging Jim Tovey Lakeview Conservation Area.

This linear park corridor will see the transformation of currently degraded and inaccessible sections of the creek into a revitalized and thriving natural feature. A restoration strategy will bring enhanced vegetated buffers that will protect and maintain the integrity and functions of this important ecosystem.

Proposed uses within the adjacent Serson Innovation Corridor will be sited and designed to ensure the integrity of the Serson Greenway is protected, with appropriate building setbacks, materials and drainage features considered.



Figure 5.3.3ccc - A sensitive approach to the integration of trails can reinforce the connection between people and the natural environment

Where the protection of sensitive features may be reasonably assured, consideration may be given to the integration of a trail feature that will allow people to experience this natural habitat. The trail can be designed as a 'nature' walk that will lead people through the Greenway to ultimately link with the meadows, forests and wetlands of the Jim Tovey Lakeview Conservation Area and the lake. The trail will be comprised of materials conducive to the protection of the corridor features.

Trailhead features may be integrated at key access points to provide seating and shade structure amenities. Information signage may be provided for wayfinding purposes, as well as to encourage users to stay on the designated path to avoid damage to sensitive environments, and education trail users on the purpose and importance of the flora and fauna features of the Greenway. The same signage may be used to describe the green energy systems and features that are a key component of the Lakeview Village community.



Figure 5.3.3ddd - Conceptual Plan of Serson Greenway

5.3.4 SUPPORTIVE OPEN SPACE - PRIVATE

Privately-owned publicly accessible open spaces are an essential component of Lakeview Village’s green network, complementing the parks and natural areas to provide continuous amenity spaces throughout the community.

Lakeview Village’s privately-owned publicly accessible spaces are generally smaller in scale and are not identified as parks by the public; however, they are intended to represent extensions of the public open space network with similar high-quality design.

Commonly shared between residents of adjacent townhomes or mid-rise buildings, these open spaces may include interior courtyards, arrival courts, and parkettes, providing an appropriate and inviting transition between the public and private realm. Landscaping features in these spaces may include a combination of trees, planting, benches, entry features, hardscaping elements, and lighting.



Figure 5.3.4a - Example of an arrival courtyard and seating area



Figure 5.3.4b - High-quality privately owned publicly accessible spaces



Figure 5.3.4c - Vegetated interior courtyard between residential buildings



Figure 5.3.5d - Interior courtyard and footpath

5.4 ACTIVE
TRANSPORTATION

The transportation system for Lakeview Village will be designed to encourage a shift away from Single Occupant Vehicle (SOV) travel, and to embrace multi-modal transportation options with an emphasis on transit and active transportation. This will reduce vehicle trip generation, reduce traffic delays, alleviate congestion, reduce energy consumption and emissions.

The Lakeview street system and the improvements currently in the planning stages for the surrounding transportation network will provide enhanced connectivity for transit, pedestrians, cyclists as well as private vehicles. It is essential to seamlessly link Lakeview Village to the neighbouring communities to achieve a cohesive fine grain network that allows for attractive and competitive route options and travel mode choice. The end results will be a community that will have a highly connected network of streets and routes for flexible and effective transit and active transportation to support walking and cycling.

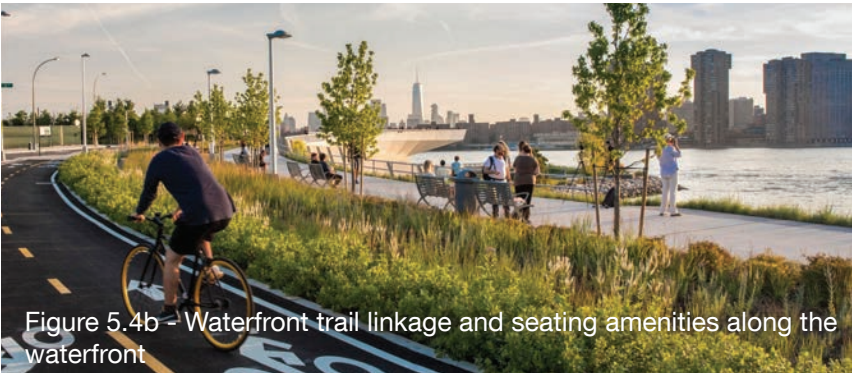


Figure 5.4b - Waterfront trail linkage and seating amenities along the waterfront



Figure 5.4c - Greenways for daily transportation needs

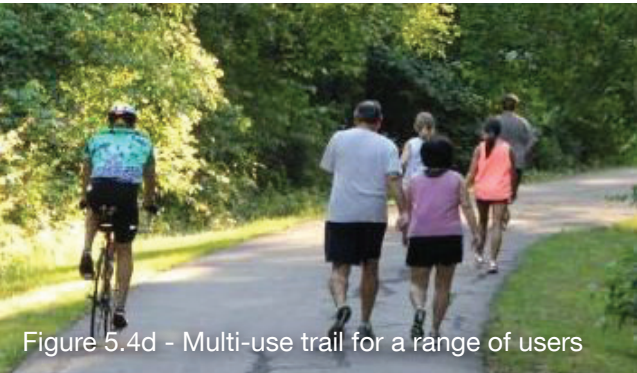


Figure 5.4d - Multi-use trail for a range of users

5.4.1 PEDESTRIAN REALM

The Lakeview Village development shall incorporate generous sidewalks and walkways as well as a unified urban design vocabulary and plentiful space for public events.

The character of the Pedestrian Realm Network shall be urban. This not only reflects the nature of the surrounding urban development, but also the fact that there are a variety of existing large parks in the immediate vicinity, such as Lakefront Promenade Park, Douglas Kennedy Park, RK McMillan Park, Marie Curtis Park and AE Crookes Park, that fulfill different functions. The Pedestrian Realm Network will also include trails (e.g., Waterfront Trail) and look-out opportunities on the existing breakwater and piers.

The Pedestrian Realm Network will be programmed for animation, recreation and all-season interest. Wayfinding signage will be provided throughout the community that directs people to the various parks within and adjacent to the waterfront, and to Lakeshore Road.

The Pedestrian Realm Network and the Street System are to be specifically designed to accentuate view corridors throughout and through the community, as well as broader panoramic views at the water’s edge.

Lakeview Village will integrate a high quality of pedestrian focused public realm throughout the proposed development that emphasizes walkability and a pedestrian scale. The pedestrian connections will provide increased permeability and accessibility. Streets will be designed to incorporate active transportation and provide views and access to the waterfront.

All streets, specifically Lakefront Promenade, New Haig Boulevard, Waterway Street, New Aviator Avenue, The Esplanade, and New Ogden Avenue, will be designed with enhanced streetscapes that may include among other things; adjacent park access, wide sidewalks, street trees, planting, and furniture.

Pedestrian connections will be seen to promote and identify existing and planned trails in Lakeview Village, including municipal connections to the existing Waterfront Trail.

The Pedestrian Realm Network will be constructed with the following attributes:

- Pedestrian facilities will be designed to the highest quality standards. Top quality building materials, low impact development standards, informed planting choices and environmental sustainability are design priorities;
- All privately owned, publicly accessible elements of the Pedestrian Realm Network will be safe, secure and accessible to the public. These elements of the Pedestrian Realm Network will include adequate signage that indicates when it is open and accessible to the public;
- Comprehensive maintenance schedules for both the public and private sectors, will be developed for the Pedestrian Realm Network to ensure safe, accessible and healthy landscapes;
- The Pedestrian Realm Network will include trees, shrubs and groundcover;

- The design of elements of the Pedestrian Realm Network will prioritize pedestrian comfort. Maximized daily sunlight and protection from wind and other elements will be considered to support year-round use;
- Pedestrian amenities, such as backed seating, tables, washrooms, water features and waste receptacles shall be of a high quality and readily available;
- The Pedestrian Realm Network will include highly visible locations for Public Art.
- The Pedestrian Realm Network will include high quality, barrier free, AODA-compliant programmable space that can accommodate the needs of users and facilitate socializing, special events and recreation;
- The Pedestrian Realm Network shall be appropriately linked with off-site pedestrian and cyclist facilities.



Figure 5.4.1a - Wide pedestrian footpaths leading into open space and view areas



Figure 5.4.1b - Active public square will serve as the focus for the community



Figure 5.4.1c - ‘Woonerf’ style street optimizing shared road space and slow driving speeds

5.5 RETAIL, CULTURE & EMPLOYMENT USES

5.5.1 RETAIL

The majority of specialty retail uses at Lakeview Village are concentrated in Lakeview Square. The Square will be a new destination for unique waterside dining and activity in Mississauga – a place to visit, play, and explore. With a mix of restaurants and active storefronts, Lakeview Square will bring the energy of Lake Ontario into Lakeview Village.

Additional retail will be located in the Marina district, which will contain Mississauga’s best waterfront restaurant, with views of the lake, marina, and skyline.

Lakeview Inlet and Inspiration Point will form a special destination with a mix of seasonal programming, kiosks, pop-up retail, and activities.

Lakeshore Gateway will provide residents of Lakeview Village and the broader Lakeview community with neighbourhood conveniences and amenities.



Figure 5.5.1a - Working bakery



Figure 5.5.1d - A variety of food and beverage options



Figure 5.5.1b - Patios and restaurants spilling into the outdoors



Figure 5.5.1c - Activity-based retail



Figure 5.5.1e - Ground storey retail mixed with mid-rise residential uses



Figure 5.5.2a - Flexible market building



Figure 5.5.2b - Preliminary concept of outdoor performance stage and amphitheatre



Figure 5.5.2c - Interactive garden with year-round accessibility

5.5.2 CULTURE

Lakeview Village supports the City of Mississauga's plans to incorporate cultural uses on city-owned lands in Lakeview Village.

This plan allows for a number of flexible future uses to be incorporated, such as a large-scale cultural destination or a smaller-scale music venue, garden, or flexible market building. LCPL is committed to continuing to work with the City and other partners to achieve the mutual goal of creating a cultural hub at Lakeview Village.



Figure 5.5.2d - Flexible community space



Figure 5.5.2d - Interactive water features



Figure 5.5.3a - Sustainably designed modern office spaces



Figure 5.5.3b - Educational uses



Figure 5.5.3c - Collaborative work spaces



Figure 5.5.3e - Hard and soft landscaped public space surrounded by employment uses

5.5.3 EMPLOYMENT USES

A key component of the Master Plan is Serson Innovation Corridor.

This district, delivered in partnership with the City of Mississauga and the Province of Ontario, is envisioned to be a future employment hub, with a potential mix of office, institutional, and educational uses. Delivering a mix of flexible office space and potential educational uses, Serson Innovation Corridor is focused on innovative industries. The corridor benefits from its proximity to the Jim Tovey Lakeview Conservation Area, a future conservation area and innovative green space, as well as Lakeview Square, Lakeview Village's retail and cultural heart.



Figure 5.5.3d - Flexible office spaces and coworking



Figure 5.5.4a - Seasonal markets



Figure 5.5.4b - Special events / food festivals



Figure 5.5.4c - Year-round activation

5.5.4 ACTIVATION

Activating public spaces and retail areas will be an important component of Lakeview Village's placemaking strategy.

With a mix of daily, weekly, monthly, seasonal, and annual events, activities and programming, Lakeview Village will be a new gathering place and hub of activity in Mississauga. Activation may include arts and cultural activities, food-based events, and regular fitness activities. Programming will occur through all four-seasons with a focus on flexibility in the public realm and the ability to close roads, especially the pedestrian-focused character streets.



Figure 5.5.4d - Health and fitness programming



Figure 5.5.4e - Arts and cultural events

5.6 DEVELOPMENT PROGRAM

The Development Master Plan generally follows the height and density distribution envisioned by the ILMP and as prescribed in the City's MOP policy planning framework.

The DMP is proposes some exceedances with respect to unit count in certain precincts but overall built form distribution generally remains the same. The justification for the units will be noted in various other sections of this Plan as it combines a balance of design considerations and technical feedback. Generally, the change in units is a modest increase reflecting an improved urban design vision for the major node and Lakeview Village, which will continue to fulfill the vision for these lands as dictated by the City's MOP.



Figure 5.6 - Preliminary distribution of height and density

5.6.1 UNIT & BUILT FORM COUNTS

The following table indicates the LCPL Master Plan proposal based on the latest built form model for the subject lands. At this time, the DMP is showing an overall unit count of just over 7,700. It should be noted that this DMP is meant to be a framework of design principles which supports site development anticipating an overall unit count of between 7,000 - 8,000 units for the Ogden Green / Cultural Waterfront precincts of the MOP. Further, the built form typology and distribution continues with the blend of low-rise, mid-rise, and taller building elements as envisioned in the MOP, although there is a slight increase in the mid-rise built form with decreases in the lower and taller building elements. The current built form distribution continues to reinforce this Major Node character area as a predominantly mid-rise community, but with enough built form variety to promote a demographic mix contributing to a complete community with various ownership and accommodation opportunities.

5.6.2 HEIGHT & DENSITY DISTRIBUTION

The DMP provides details on both the height and density distribution for each precinct as well as by block.

General height and density considerations and applications are as follows:

HEIGHT

- The tallest buildings will be located in the Marina District of the Cultural Waterfront precinct as defined by the City. The Cultural Waterfront precinct is recognized by the City as the area to have the tallest buildings, with a 25-storey limit and additional height subject to a height study. The tallest buildings are separated from existing residential communities with a distance of at least 600 metres;

Proposed Distribution of Housing and Unit Targets for LCPL Lands

		Townhouses		Mid-Rise Buildings		Taller Buildings	
Precinct	Total Units	Number	% of Total Built Form	Number	% of Total Built Form	Number	% of Total Built Form
Ogden Green, Cultural Waterfront	7,754	749	10	4,943	64	2,062	26
	6,800	1,020	15	3,400	50	2,380	35

xxx - Proposed Number in DMP
xxx - Proposed Number in City MOP (As per City of MOP Policy 13.4.8.3.6 Table 1: Distribution of Housing and Unit Targets by Precinct)

- Taller buildings are generally located around the future enhanced transit route and select locations to fulfill design objectives for the overall community;
- Mid-rise buildings are the predominant built form closest to Lake Ontario, as well as a built form frame to a majority of residential blocks;
- Proposed height exceedances beyond the City MOP policy framework are limited to very specific locations with the justification noted in further sections of this DMP;
- The lowest heights, comprised of predominantly townhouses, are generally located north of the Waterway Common (west/east park). This built form is located in either the centre of residential blocks or in stand-alone blocks of a mix of townhouse type built forms;
- The range of heights proposed mimic comparable developments in other City Major Node locations;
- Height transitions are purposely designed for intra- and inter-block considerations. Details on those parameters are described in other sections of this DMP and in particular, to address City requirements regarding sun / shadow, wind, and other technical considerations;
- Podiums and tower floorplate sizing intentionally introduces visual calm, both at street level and for skyviews, to ensure heights are perceived as minimal as possible;
- Overall height considerations provide a logical fit with future development scenarios within adjacent Rangeview Estates precinct lands, allowing for the continuation of the overall Major Node completion.
- Height along Lakeshore Road East is expressed through a podium with a taller building element set back from the street.

DENSITY

- The DMP proposes the highest densities along the western edge of the subject lands and in proximity to Waterway Common and Lakeview Square, which generally match the height distribution;
- Higher densities, with the exception of the Marina District, are closely located to the enhanced transit route and / or the collector roads within the LCPL lands. This will reinforce the feasibility of transit usage and connecting links with multi-modal opportunities;
- The highest density locations will take advantage of the proximity and easy access to parkland, commercial uses, and cultural infrastructure, as well as job opportunities;
- Like height, density lowers along the northern edges to address logical fit considerations with the future development of Rangeview Estates.

5.7 BUILT FORM

With a single master developer having control over a large site, Lakeview Village has the unique opportunity to design and shape the built form as a legacy community that transcends what is possible only utilizing policies. The goal is to achieve the ambitious vision for this mixed-use community set by the City in this Inspiration Lakeview Master Plan adopted in 2014 and the Official Plan in 2017.

The experience of the public realm within and surrounding Lakeview Village played a key role in informing the location of heights and density. Viewsheds, vistas, and sight lines were studied from various locations in Lakeview Village, surrounding areas in the city and from more distant locations. The goal was to creatively use built form to accentuate primary viewsheds to the horizon line of the lake, framing the expanse of the sky from the public realm, and enhancing the breadth and depth of open spaces.

This Development Master Plan embraces the goals of the City’s collective policies for building typologies, density, scale, massing and height for this Major Node Character Area. Strict policy adherence has been balanced with our commitment to developing Lakeview Village as a unique legacy community. This mandates that the built form of Lakeview Village be designed to deliberately avoid many typical characteristics exhibited in the GTA’s previously developed city centres, major nodes and along the Lake Ontario shoreline that have not achieved the sense of place envisioned by the community for this site.

The built form massing of Lakeview Village has been carefully and purposefully designed in accordance with the following seven principles:

- 1. **Diversity in Design:** Sculpt unique building forms to juxtapose massing, punctuate a diverse skyline, and avoid monotony in a predominantly mid-rise community.
- 2. **Vistas & Views:** Frame primary vistas to accentuate viewsheds of the lake, sky, and public spaces, with terminated views focused internally.
- 3. **Design for Community:** Achieve human scale on each building by defining a base, utilizing expression lines and step backs for transitions, and articulating a cap and roofscape profile.
- 4. **Frame Outdoor Rooms:** Integrate facades with the public realm to enclose a diverse series of ‘outdoor rooms’ that address wind, thermal comfort, and shadows.
- 5. **Activate Placemaking:** Program density to strategically generate placemaking activity in public spaces, along transit routes, and on the lakefront.
- 6. **Landmarks & Wayfinding:** Establish vertical landmarks to serve as points of orientation and wayfinding that connect Lakeview Village, the city, and the region to the lakefront.
- 7. **Distinction & Difference:** Shape varied compositions of blocks that organically differentiate each district and integrate sustainability, constructability and affordability.



Figure 5.7a - Lakeview Village's southern districts and associated built form

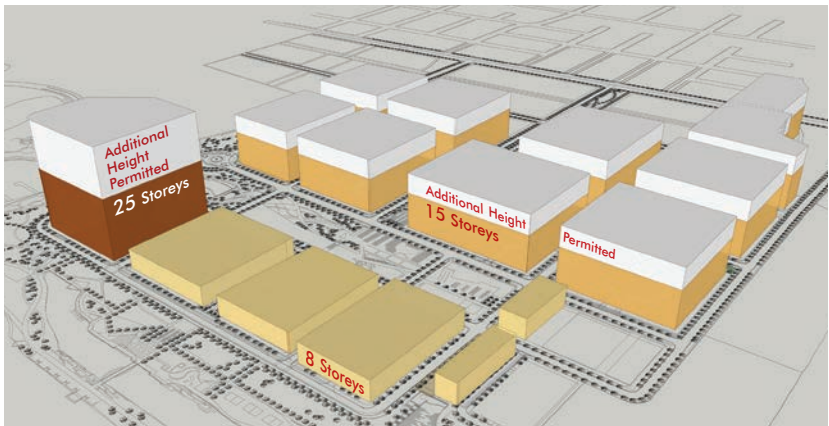


Figure 5.7b - Diagram illustrating OPA permissions for maximum height by district at Lakeview Village

PRINCIPLE 1:

Diversity in Design: Sculpt unique building forms to juxtapose massing, punctuate a diverse skyline, and avoid monotony in a predominantly mid-rise community.

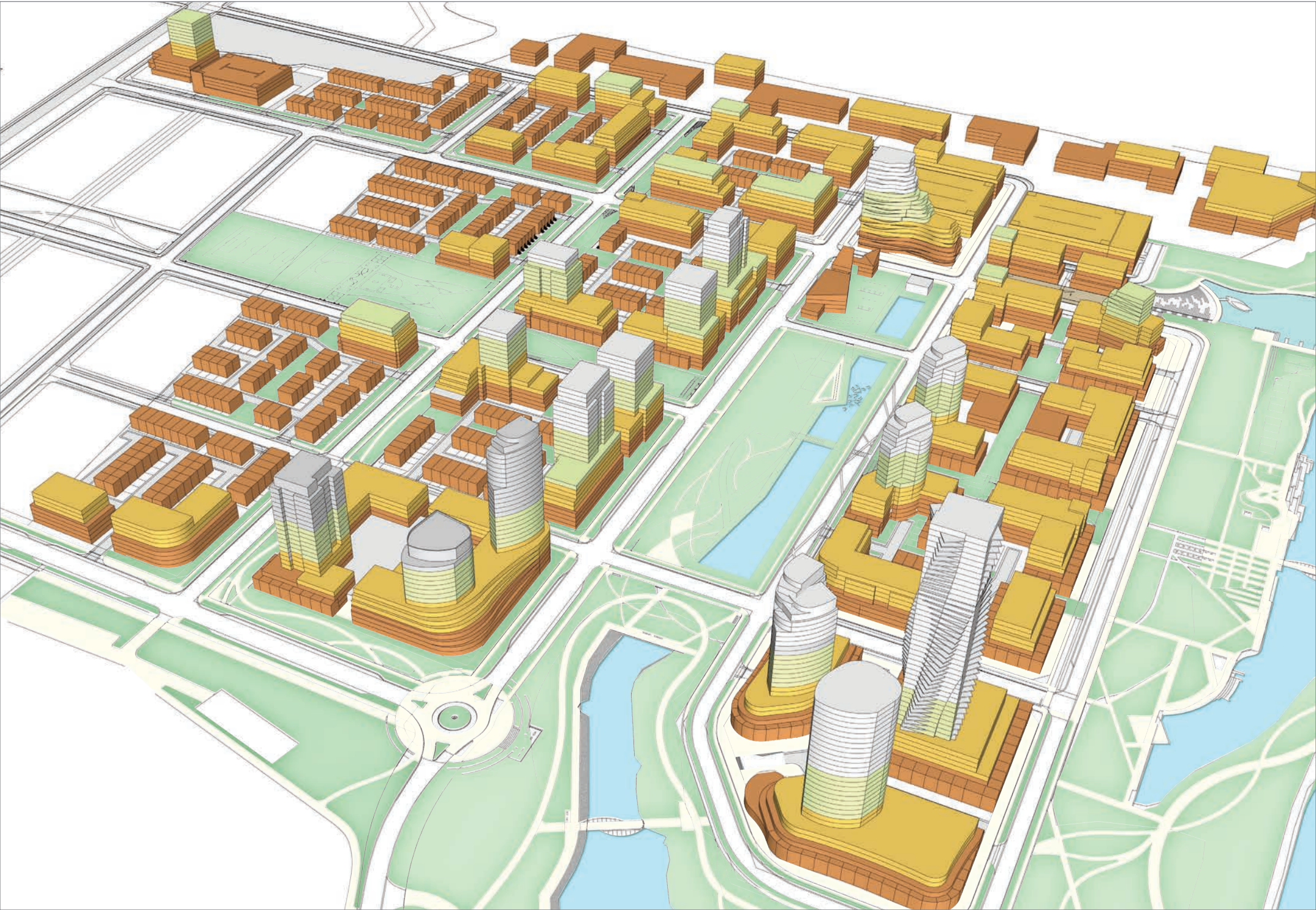
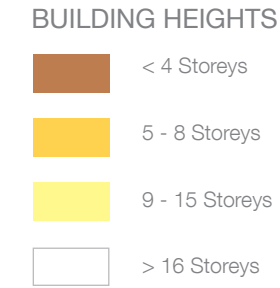


Figure 5.7c - To reduce monotonous repetition in a predominantly mid-rise community, Lakeview Village contains a variety of blocks that are differentiated by massing that juxtaposes building typologies and height to create a diverse roofscape and skyline profile for the community.



Figure 5.7d - There are a range of building forms, heights, and spaces spanning the lakefront along Inspiration Point from the Marina District to Lakeview Square.



PRINCIPLE 2:

Vistas & Views: Frame primary vistas to accentuate viewsheds of the lake, sky, and public spaces, with terminated views focused internally.

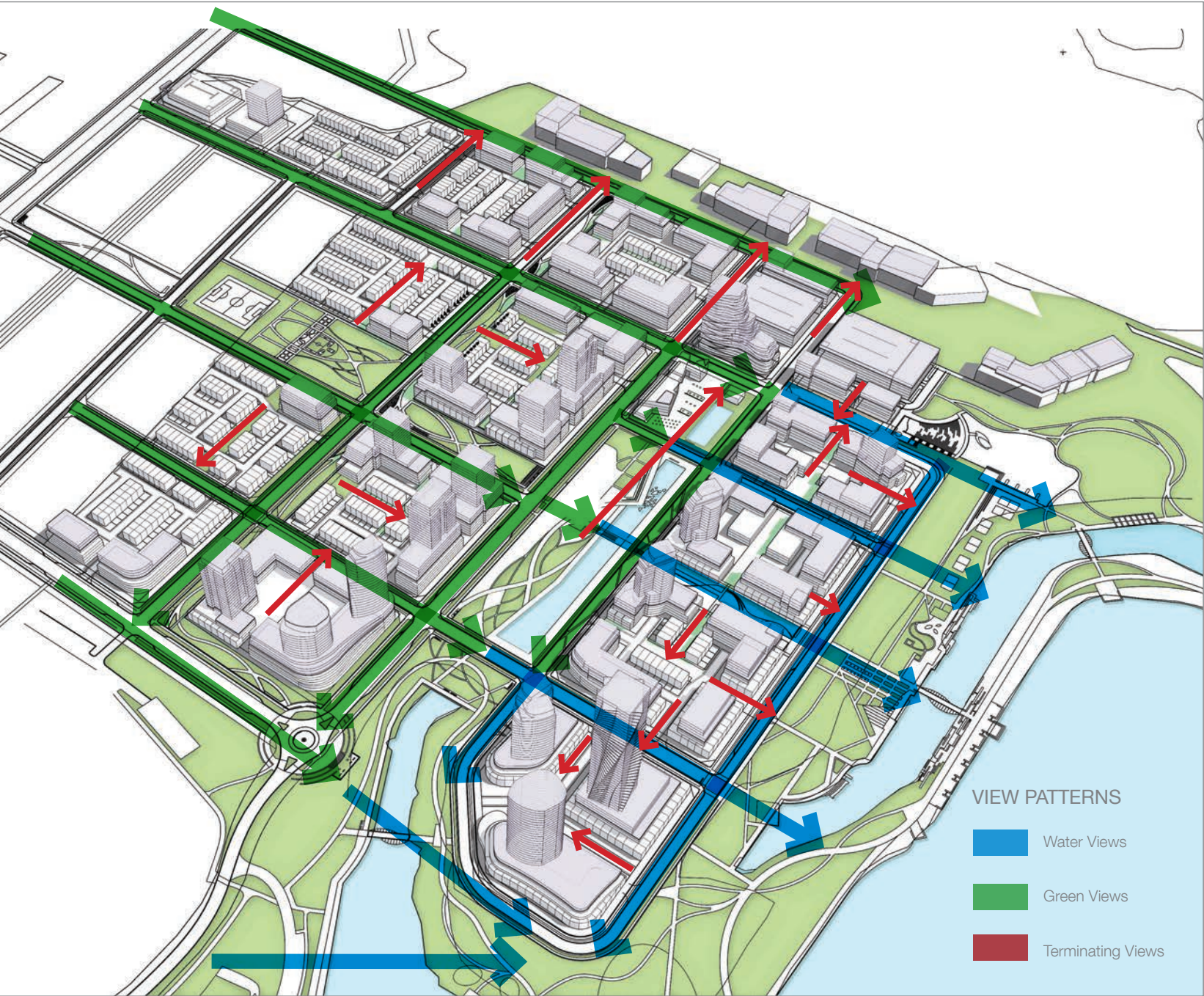


Figure 5.7e - The expansiveness of the primary lake, sky and open space viewsheds can be contrasted with secondary sight lines focused on spatial enclosures and terminated views focused more internal within the community.



Figure 5.7f - Lakeview Village uses the massing of built form as a backdrop to frame viewsheds of the lake, the sky and public spaces from within a public realm that accentuates the vista experience for residents and visitors.

PRINCIPLE 3:

Design for Community: Achieve human scale on each building by defining a base, utilizing expression lines and step backs for transitions, and articulating a cap and roofscape profile.

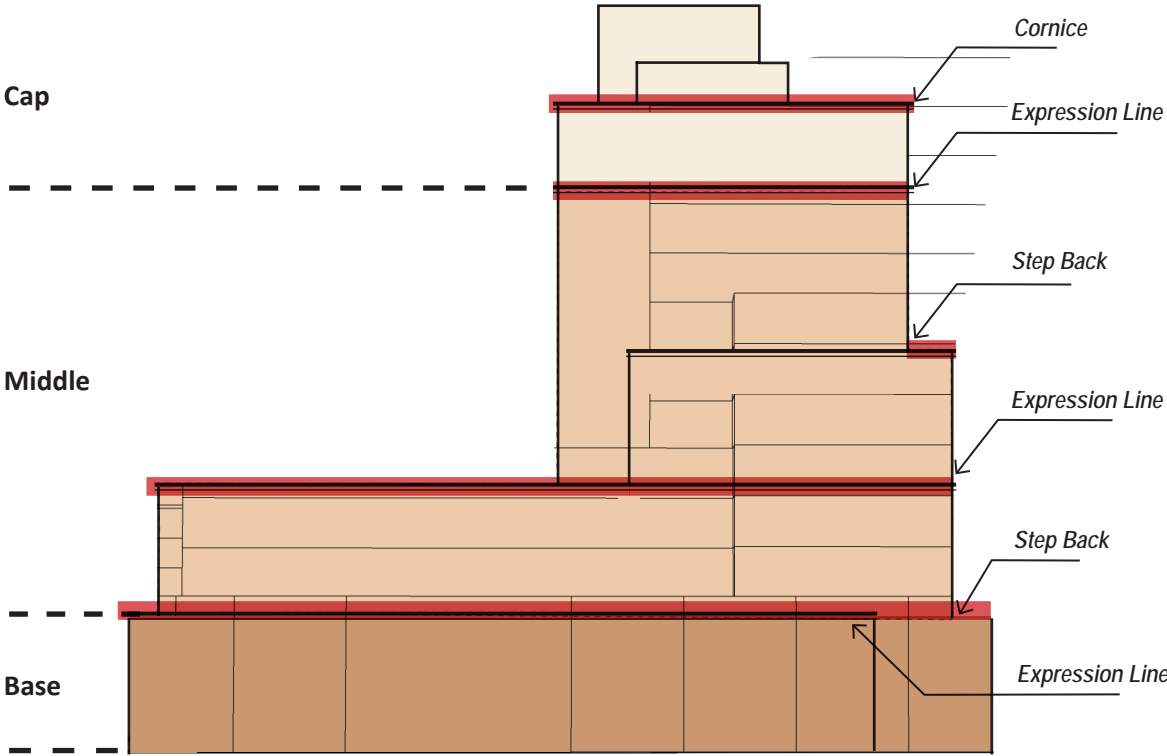


Figure 5.7g - Illustration of how the massing of a building will be defined into a base, middle and cap with expressions lines and step backs for transitions.

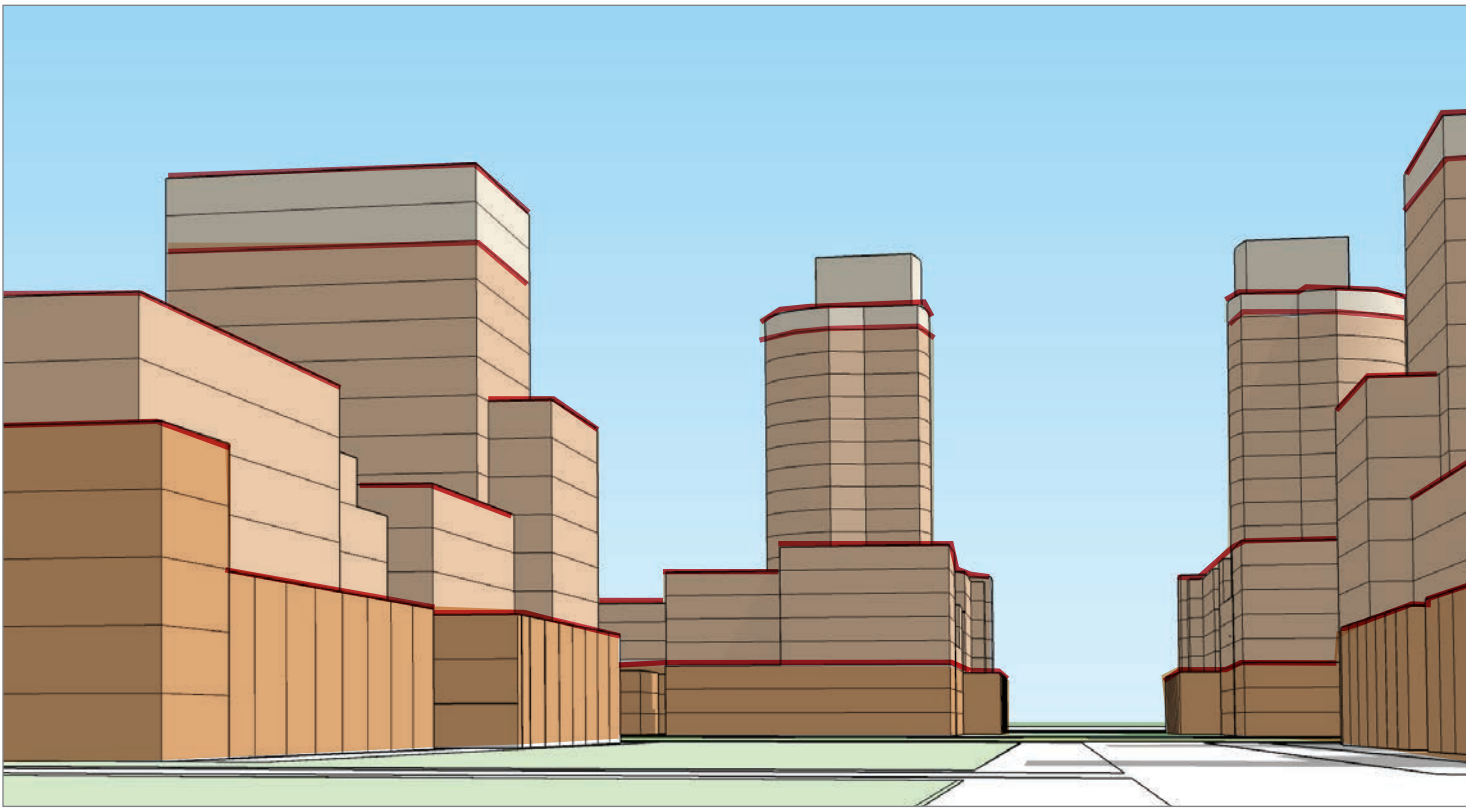


Figure 5.7i - To avoid monotonous repetition in a predominantly mid-rise community, a variety of blocks that are differentiated by massing that juxtaposes building typologies and height to create a diverse roofscape and skyline profile for the community.

PRINCIPLE 4:
Frame Outdoor Rooms:
Integrate facades with the public realm to enclose a diverse series of ‘outdoor rooms’ that address wind, thermal comfort, and shadows.



Figure 5.7k to l - Examples of commercial and residential facades integrated with the public realm that enliven the pedestrian-friendly experience.

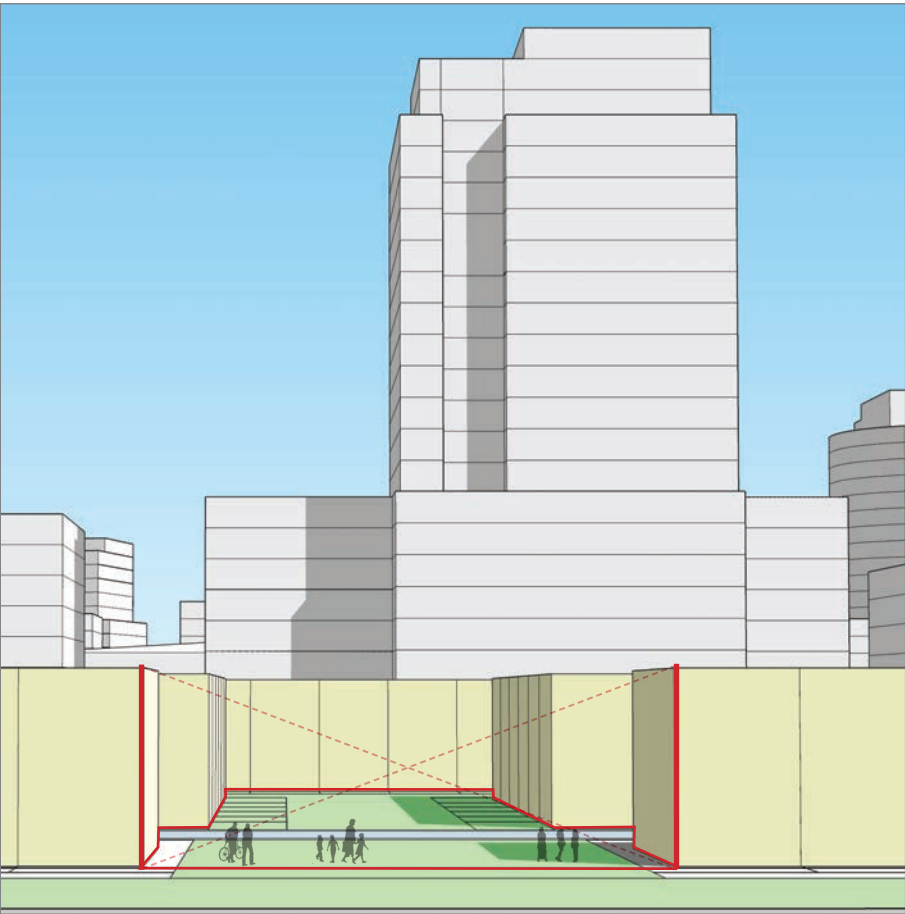


Figure 5.7j - Lakeview Village contains a series of enclosed mid-block mews and courtyards that have more intimate height-to-width ratios.



Figure 5.7m - Integrating the base of buildings with the edge of the public realm creates 'outdoor rooms' that are inviting and comfortable for people.

PRINCIPLE 5:

Activating Placemaking: Program density to strategically generate placemaking activity in public spaces, along transit routes, and on the lakefront.



Figure 5.7n - Density at Lakeview Village is strategically programmed to generate placemaking activity in public spaces, the targeted cultural and amenity areas, the lakefront, and along transit routes.



Figure 5.7o - Plan illustrating generalized density patterns by block

PRINCIPLE 6:

Landmarks & Wayfinding: Establish vertical landmarks to serve as points of orientation and wayfinding that connect Lakeview Village, the city, and the region to the lakefront.



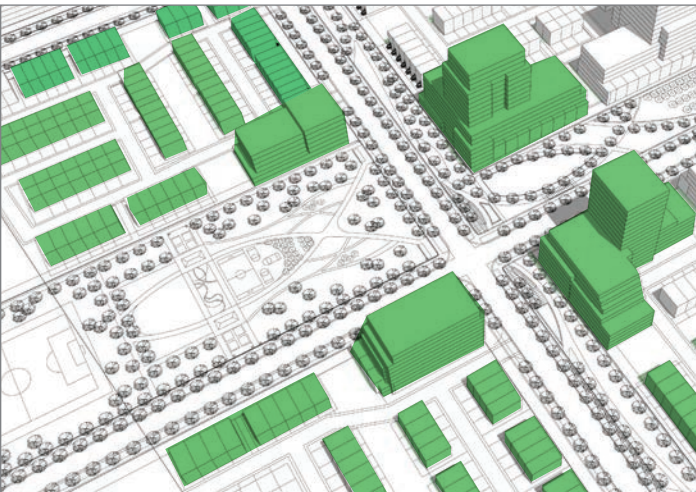
Figure 5.7p - Vertical landmarks can help mark the location of the lakefront, framing views and guiding the public toward the water's edge without cordoning off the waterfront with the all too typical 'wall of towers' along the lake.



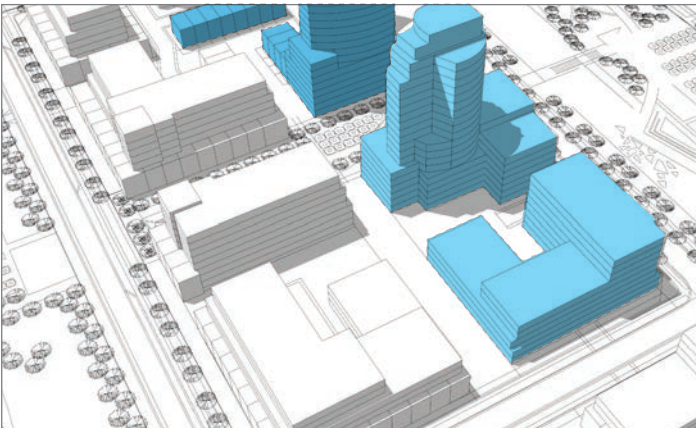
Figure 5.7q - Lakeview Village's massing will establish vertical landmarks and wayfinding that connect Mississauga to the lake

PRINCIPLE 7:

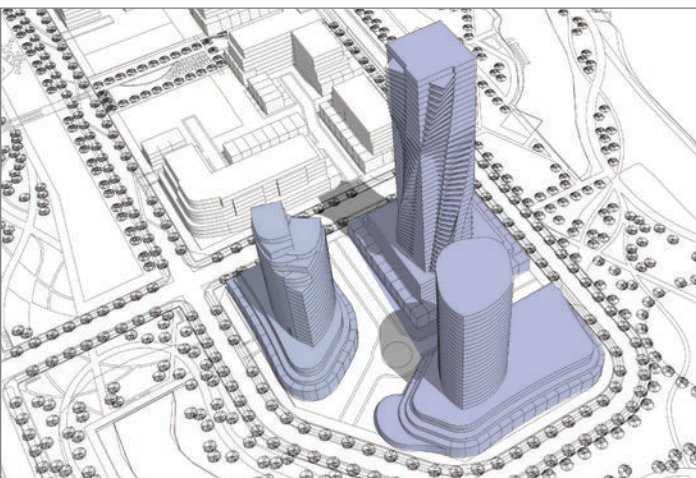
Distinction & Difference: Shape varied compositions of blocks that organically differentiate each district and integrate sustainability, constructability and affordability.



Ogden Green



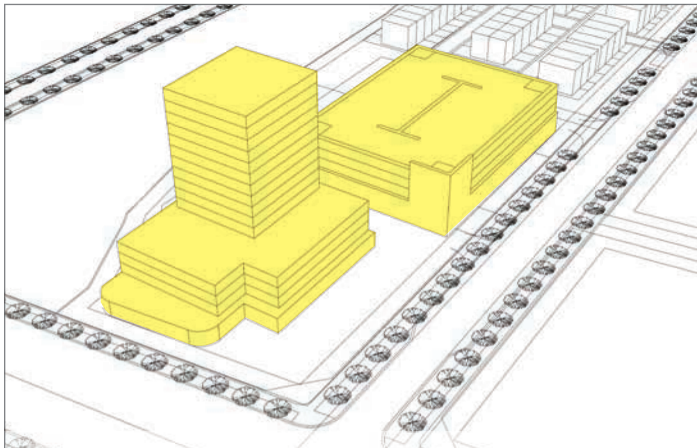
Waterway Common



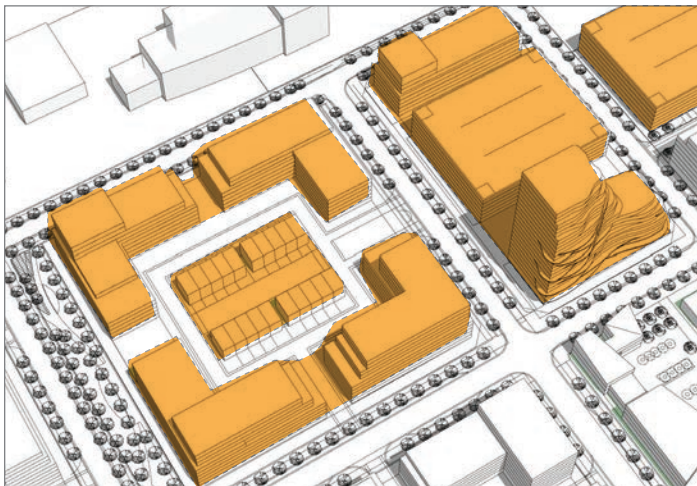
The Marina



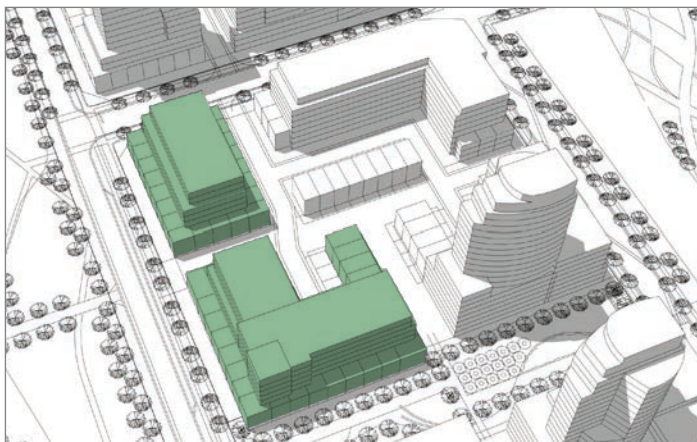
Figure 5.7r - The identity of each district of Lakeview Village is shaped by unique compositions of blocks and buildings. This creates a richly textured urban fabric that emulates the organic evolution of communities that is shaped by many hands over generations while integrating state-of-the-art constructability, sustainability and affordability.



Lakeshore Gateway



Lakeview Square



Inspiration Point

5.7.1 RESIDENTIAL BUILDING
TYPOLOGIES AND BLOCKS

Lakeview Village incorporates a diverse range of residential building typologies to provide a variety of massing, height and scale in this predominantly mid-rise community. Monotony and sameness are avoided by providing organic compositions of built form that sculpt unique blocks to differentiate the identity for each district and neighbourhood.

Some blocks have a highly consistent built form massing while others contain transitions and juxtapositions of vastly different built form massing and height. Block compositions can be complex with larger mixed-use buildings lining the edge of the major street network and smaller-scale townhouses set intimately around a fine-grain network of green spaces at the interior of blocks. The patterns of buildings along a network of fine-grain streets and open spaces creates a multitude of short blocks and corner conditions which creates a built form framework that creates a more interesting pedestrian realm.

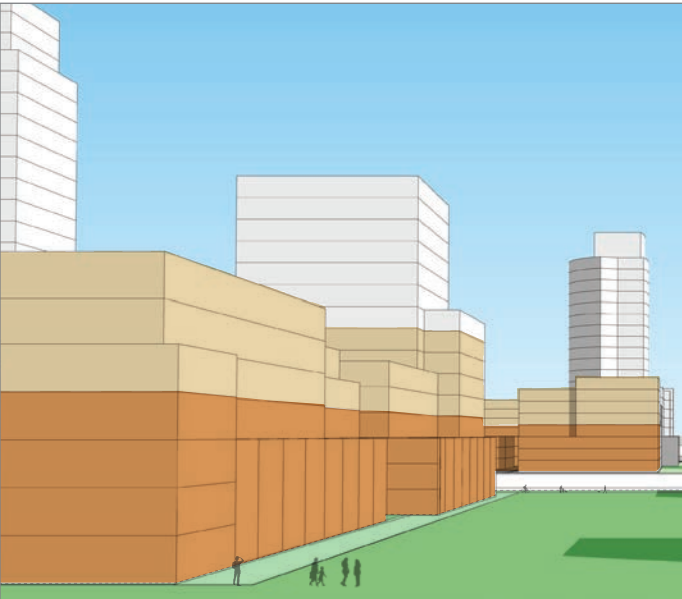


Figure 5.7.1a - Ground level view illustrating how the diversity of massing can help avoid monotony from the public realm.



Figure 5.7.1b - To avoid the monotony and sameness in a predominantly mid-rise community, Lakeview Village is shaped with organic compositions incorporating a diverse range of building types to achieve a variety of massing, height and scale.

Townhouses and Low-Rise Buildings - up to four storeys in height

Townhouses and low-rise buildings are used to establish the residential scale along the streetscape in majority of blocks in Lakeview Village. This building type includes a wide range of ground-related townhouses as well as townhouses that are a component of blocks containing mid-rise and taller buildings. An essential function of townhouses and low-rise apartment buildings is to aid in stepping-down the height and transitioning the scale along the base of mid-rise and taller buildings, and framing blocks at a human-scale along streets and open spaces including mews.

The façade of townhouse and low-rise buildings types along the street and public spaces is defined by a base, middle and cap:

- Base - defined from the ground plane to a horizontal line on the lower façade such as a water table, window sill or the entire ground floor level.
- Middle wall - defined by wall height from top of base to bottom of cap and articulated by fenestration, projections, and recesses.
- Cap - defined at the top of the building by a cornice line or roof eaves and may include roof, dormers, and parapets.

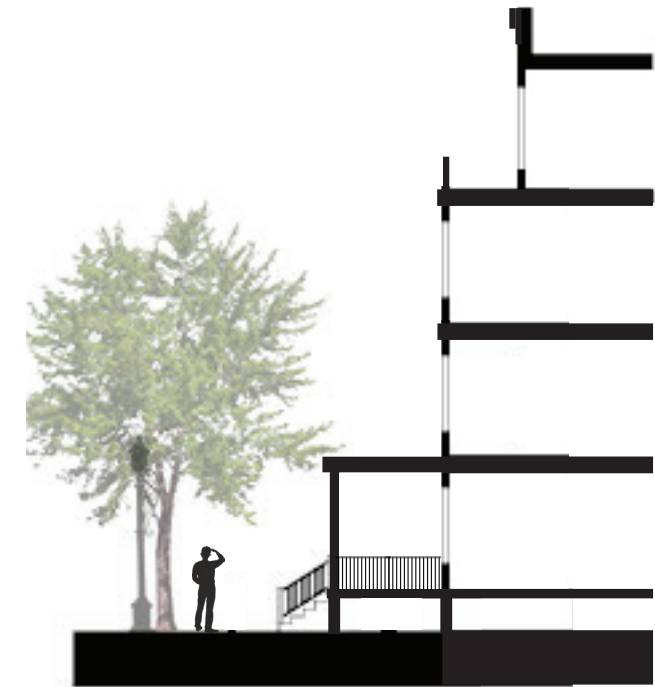


Figure 5.7.1c - Section view illustrating how townhouse and low-rise building is defined by a base, middle wall and cap along a street or public space.

Townhouses and low-rise buildings help establish a continuous human-scale façade for most residential buildings along the edge of blocks and open spaces to frame the enclosure of a diverse series of 'outdoor rooms'. Essential to framing the outdoor rooms is articulating the façade with projections and recesses to create depth and shadow. This makes for a more pedestrian-friendly experience on sidewalks and walkways and also enhances the livability for residents by extending interior living areas to outdoor space, capturing views and providing shading that can aid in privacy from the street. Projections and recesses include functional and usable covered stoops, porches, canopies, verandas, bays, balconies, terraces, and other architectural elements.



Figure 5.7.1d to e - The above images illustrate how human-scale elements, such as projections and recesses like stoops and canopies, create depth and shadow that enhances framing the enclosure of an 'outdoor room' along the public realm.

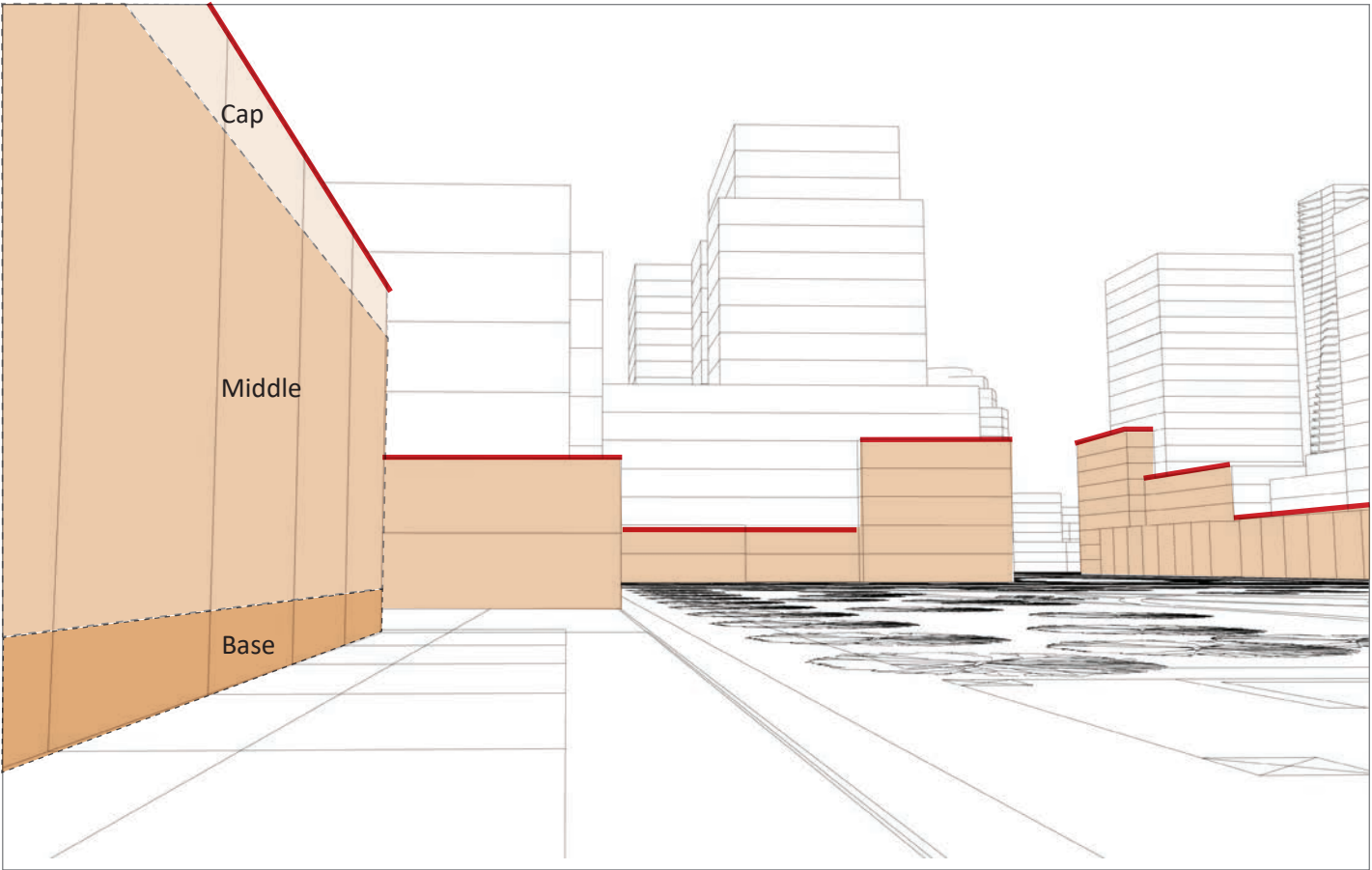


Figure 5.7.1f - Townhouses and low-rise buildings establish residential scale and aid in stepping-down and transitioning the height of mid-rise and taller buildings in Lakeview Village.

Mid-Rise Buildings – 5 to 8 storeys in height

As a predominantly mid-rise community, most of the blocks in Lakeview Village incorporate built form including mid-rise buildings. Mid-rise buildings include a range of typologies including configurations that can be attached to or share the block with low-rise or taller buildings resulting in the juxtaposition of height and punctuation of roofscape profiles that avoids monotony of massing on the streetscape. Blocks containing mid-rise buildings have been configured in a variety of forms to aid in stepping-down the height and scale taller buildings, transitioning between building types, and establishing the appropriate height to proportionately frame larger-scale open spaces to create a sense of enclosure.

The façade of mid-rise building types along the street and public spaces is defined by a base, middle, expression line and cap:

- Base - defined from the ground plane to a horizontal line on the lower façade such as a water table, window sill or the entire ground floor level.
- Middle wall - defined by wall height from top of base to bottom of cap and articulated by fenestration, projections, and recesses.
- Low-rise expression line – defined by a belt course, change in materials or colours, or use of a terrace or step-back that provides visual continuity with the scale of adjacent or nearby townhouse and low-rise buildings.
- Cap - defined at the top of the building by a cornice line, an articulated upper floor, and parapets.

The massing of mid-rise buildings has been strategically located to enhance the framing of viewsheds from the public realm to the lake, sky and public spaces. Mid-rise buildings with a façade integrated with the public realm are essential to creating a comfortable sense of enclosure for larger-scale 'outdoor rooms' such as Ogden Green, Aviator Park, Lakeview Promenade, Waterway Commons, Inspiration Point and the lakefront. The facades of mid-rise buildings will be articulated with projections and recesses that create depth and shadow to enhance the human-scale from the public realm. Covered stoops, porches, canopies, verandas, balconies and terraces can functionally extend interior living areas to outdoor space, thus enhancing the livability for residents and providing views that connect residents to open spaces.

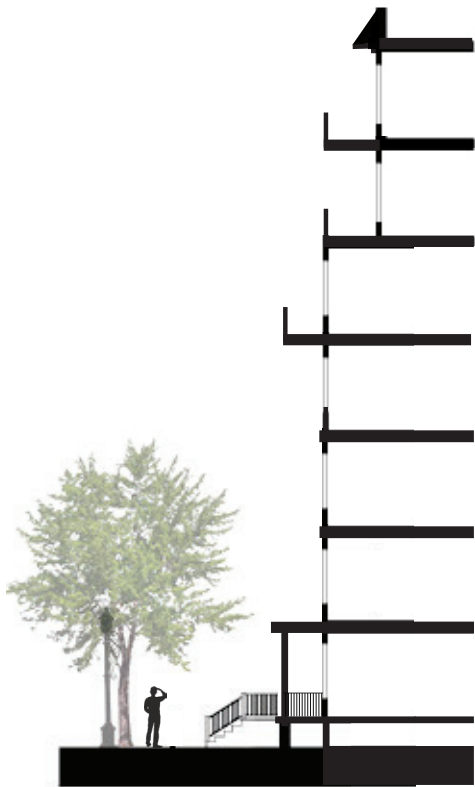


Figure 5.7.1g - Human-scale elements, such as projections and recesses like stoops and canopies, create depth and shadow that enhances framing the enclosure of an 'outdoor room' along the public realm.



Figure 5.7.1h - Mid-rise buildings with façades integrated with the public realm are essential to creating a comfortable sense of enclosure for larger-scale 'outdoor rooms' and the framing of viewsheds of the lake, sky, and public spaces.



Figure 5.7.1i - Mid-rise buildings have been configured in a variety of forms to aid in stepping-down the height and scale of taller buildings and transitioning between building types.

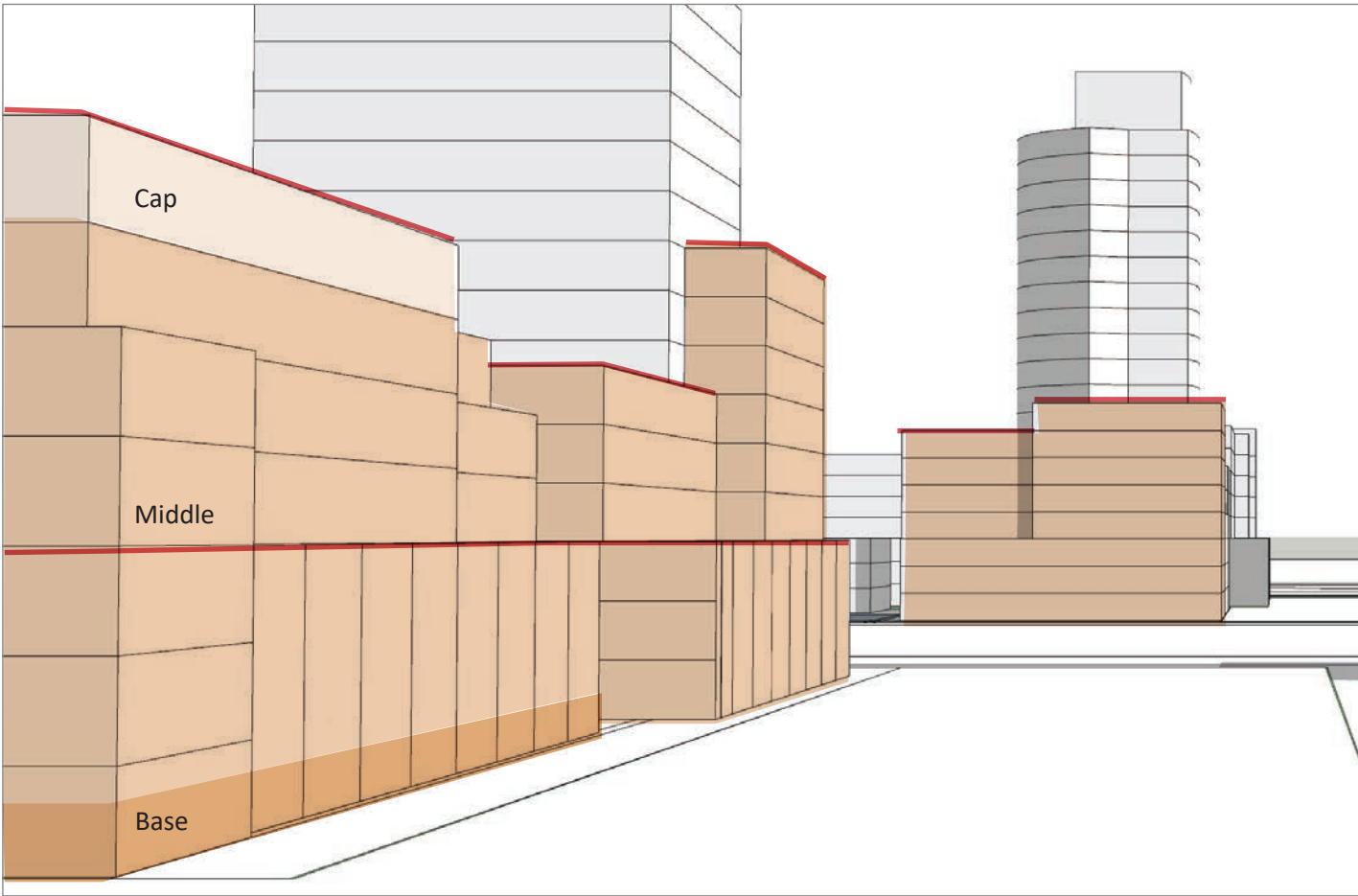


Figure 5.7.1j - Mid-rise buildings include a range of typologies and configurations that can be attached to, or share the block, with low-rise or taller buildings creating juxtaposition of height and punctuation of roofscape profiles that reduces monotonous massing along a streetscape.

Taller Buildings – 9 to 15 storeys in height and 16 storeys or taller in height

While Lakeview Village is a predominantly mid-rise community, taller buildings are anticipated by the City’s prior planning and policies for this major node. In support of the City’s vision for this site, this Development Master Plan has strategically located taller buildings and related density to enhance placemaking, wayfinding, landmarking and activation of the public realm. Lakeview Village’s taller buildings can be grouped into two categories – buildings 9 to 15 storeys in height and buildings 16 storeys or taller in height.

Taller buildings have been embedded in blocks with stepped down built form that includes lower-scale transitional massing and mid-rise buildings to juxtapose massing, punctuate roofscape profiles and avoid overbearing massing. Taller building configurations include podiums or projecting mid-rise and low-rise forms that can be attached to or share the block with the taller buildings. Blocks containing taller buildings have been uniquely configured in a variety of forms to aid in stepping-down the height and scale, transitioning between building types, and establishing the form of massing and proportion of tower shafts in relation views from streets and open spaces.

The façade of taller building types along the street and public spaces is defined by a base, middle, expression line and cap:

- Base – (on the podium or projecting mid-rise or low-rise form) defined from the ground plane to a horizontal line on the lower façade such as a water table, window sill or the entire ground floor level.
- Lower wall - defined by wall height from top of base to bottom of the low-rise expression line and articulated by fenestration, projections and recesses.
- Low-rise expression line – defined by a belt course, change in materials or colors, or use of a terrace or step-back that provides visual continuity with the scale of adjacent or nearby townhouse and low-rise buildings.
- Mid-rise expression line - – defined by a belt course, change in materials or colors, or use of a terrace or step-back that provides visual continuity with the scale of adjacent or nearby mid-rise buildings.
- Shaft - defined by a wall stepped back from top of mid-rise expression line, extending to bottom of the cap and articulated by fenestration, projections and recesses.
- Cap - defined at the top of the building by a cornice line, articulated upper floors, parapets or an ornamental form.

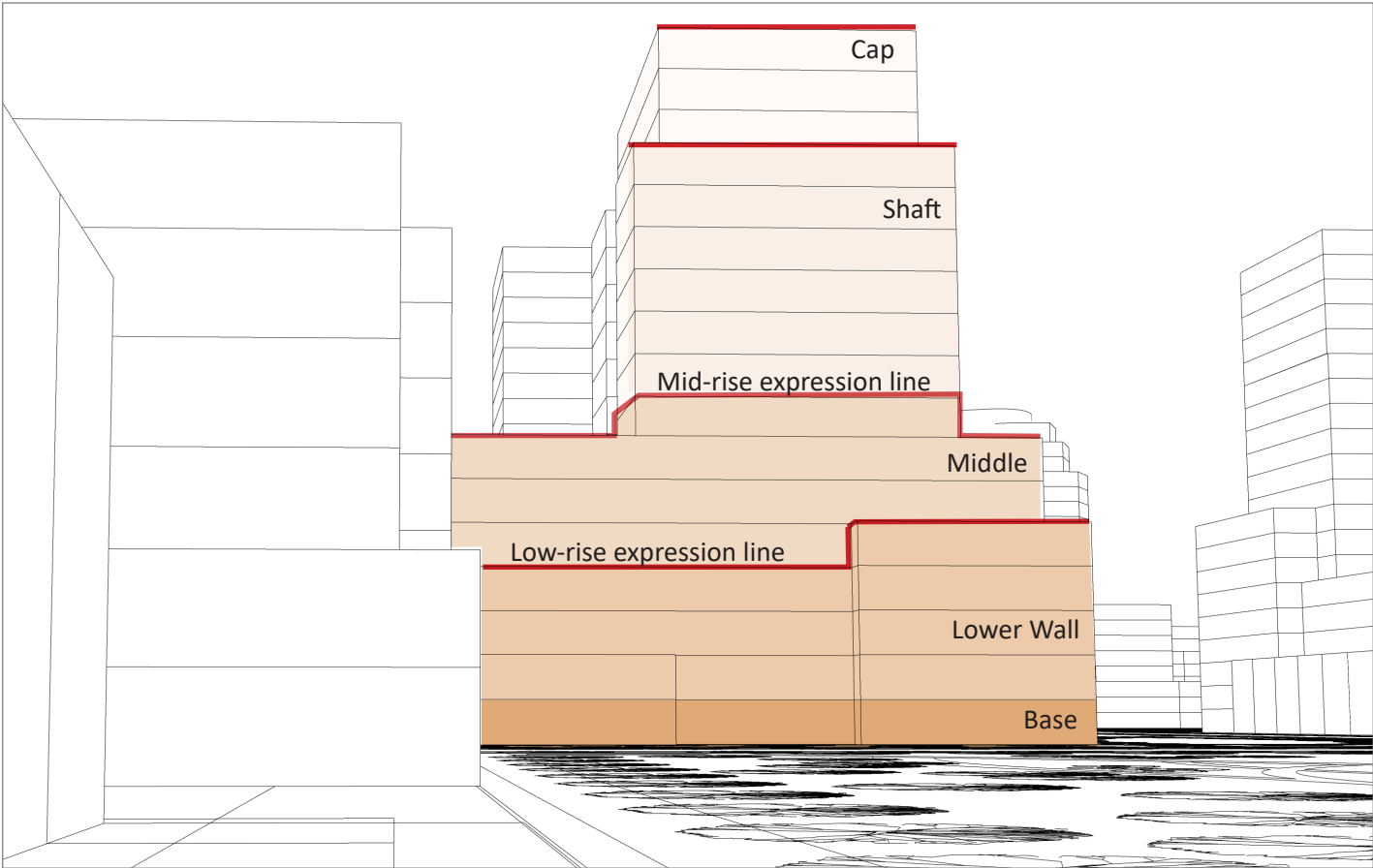


Figure 5.7.1k - Taller buildings have been embedded in blocks with stepped down built form that includes lower-scale transitional massing, such as podiums or projecting mid-rise and low-rise forms, and punctuated roofscape profiles to avoid overbearing massing.

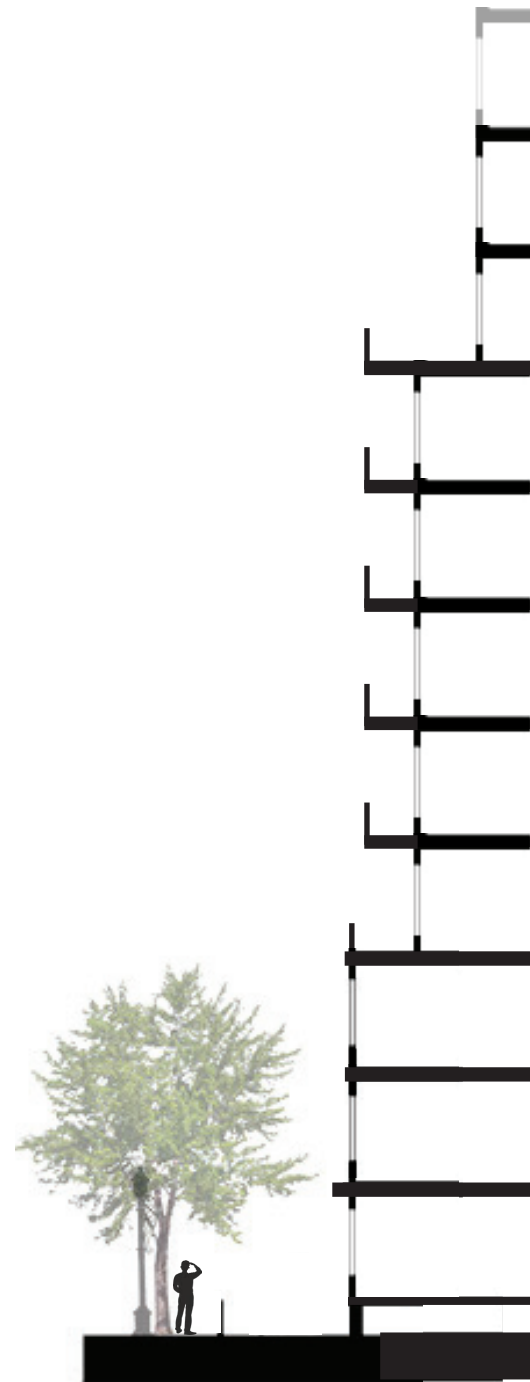


Figure 5.7.1l - Section view illustrating how taller a building can have a human-scale along a street or public space with a low-rise base and a step back on the middle wall.



Figure 5.7.1m - Transitional massing incorporating low-rise forms along the street can maintain human-scale along a block containing a taller building.

The calculated positioning of taller buildings in selected blocks is designed to separate towers further apart and sculpt a unique organic composition of massing. Throughout Lakeview Village, a visually cohesive and comfortable transition to lower elevations and the ground plane will be achieved by attached and / or adjacent mid-rise and low-rise buildings. The facades on the podium or projecting base of taller buildings will be articulated with projections and recesses to create depth making for a more pedestrian-friendly experience on sidewalks and walkways. This also enhances the livability for residents on lower floors by extending living areas to outdoor space, capturing views and providing shading that can aid in privacy from the street.



Figure 5.7.1n - At the street level, taller buildings will utilize a consistent design vocabulary with low- and mid-rise buildings by incorporating projections and recesses including covered stoops, porches, verandas, balconies and terraces which establish a pedestrian scale and reinforce neighborhood character.

5.7.2 VIEWSHEDS, HEIGHTS, & DENSITY

The Lakeview Waterfront Major Node Character Area Policies are intended to create a community that is predominantly mid-rise in form with both shorter and taller buildings to provide a diverse skyline and avoid monotonous building sameness. After careful study, taller buildings were placed in locations allowing height to serve as vertical landmarks, points of orientation and wayfinding that connect Lakeview Village, the city, and the region to the lakefront. Density has been programmed in strategic locations to generate placemaking activity in public spaces, the targeted cultural and amenity areas, the lakefront and along transit routes or within a comfortable walking distance of transit stops to promote increased transit ridership.

The experience of the public realm within and surrounding Lakeview Village played a key role informing the location of heights and density. Viewsheds, vistas, and sight lines were studied from various locations in Lakeview Village, surrounding areas in the city and from more distant locations. This analysis included fixed viewpoints as well as sequential vistas experienced as one traverses a route, sometimes referred to as the 'fourth dimension' of moving through the environment. The placement of taller buildings was devised in concert with streets and open spaces with a goal of accentuating primary viewsheds to the horizon line of the lake, framing the expanse of the sky from the public realm, and enhancing the breadth and depth of open spaces. Secondary sight lines focused on spatial enclosures and terminated views which provide contrast to the expansiveness of the primary lake, sky and open space viewsheds.



Figure 5.7.2a - To ensure the goal of becoming a legacy community, buildings were studied and placed in locations to serve as vertical landmarks, points of orientation and wayfinding that connect the entire city to the lakefront, providing density in strategic locations to generate placemaking activity in public spaces, and in response to wind, thermal and shadow analysis.



The vista, viewshed and sight line analysis is part of the rationale for proposing specific taller buildings that add distinguish a diverse skyline with the premise of 'earning the sky'. The location and proximity of taller buildings was carefully calculated to preserve ample visual permeability from within the community and the surrounding areas of the city.

At the street, block and neighborhood level, massing and height was analyzed in terms of wind, thermal and shadow impacts on the experience of the public realm. Based on initial wind, thermal and shadow analysis, the location of massing and height was repositioned and adjusted in the community to mitigate negative impacts from wind and thermal conditions at the pedestrian scale, control shadows on public space and promote wind, thermal and sunlight comfort for users of the public realm.

There is also a unique identity value to providing varying height to a predominately mid-rise community. Taller buildings provide greater design creativity in sculpting building forms to juxtapose massing, punctuate roofscape profiles and avoid monotony. This allows Lakeview Village to shape unique compositions of buildings that organically differentiate each phase of development and create a distinct identity for each district and neighbourhood. This is critical to making Lakeview Village unique in a region where there is much new development that appears repetitive and lacking in differentiation.

With a single master developer having control over a large site, Lakeview Village has been purposely designed to distinguish itself from the massing typically found in a city centre or along portions of the shoreline along Lake Ontario. Lakeview Village shapes unique compositions of buildings that organically differentiate each phase of development and create a distinct identity for each district and neighbourhood. This is critical to making Lakeview Village unique in a region where there is much new development that appears repetitive and lacking in differentiation. The placement of height and density at Lakeview Village deliberately avoids concentrating a 'cluster core' of towers competing in proximity with limited site lines and overshadowing or cordoning off the waterfront with the all too typical 'wall of towers' facing the lake.

Figure 5.7.2b - The expansiveness of the primary lake, sky, and open space viewsheds is distinguished by a diverse skyline

5.7.3 RETAIL FACILITIES & AMENITIES

Land uses that will activate the mixed-use commercial locations include retail, restaurants, services and hotels. These uses may be incorporated at the ground level of any building type – low-rise, mid-rise and taller buildings as well as be situated in free-standing kiosks, stalls or other pop-up structures. Retail facilities should be designed to reinforce a lively and transparent street or public space frontage except in specialized 'tuck away' locations. Storefronts are encouraged to have tall floor to ceiling heights, extensive glazing, iconic signage and may be designed to extend interior restaurant space to outdoor dining patios. Service locations for loading, refuse and recycling should be located 'back of the house' with access from a rear lane or minor street.

Retail facilities should be designed to reinforce a lively and transparent street or public space frontage with storefronts containing extensive glazing, tall floor-to-ceiling heights, iconic signage, and interior restaurant space that extends to outdoor dining patios.



Figure 5.7.3a - Tall floor-to-ceiling retail heights



Figure 5.7.3b - Built form that activates a lively street and public space



Figure 5.7.3c - Outdoor dining patios



Figure 5.7.3d - A restaurant extending to outdoor dining space

5.7.4 CULTURAL FACILITIES & AMENITIES

Cultural facilities such as theatres, museums, recreation centers, educational institutions, and other public and quasi-public facilities often require larger footprint and building volume to house such activities. Cultural facilities and amenities should also take advantage of key locations as activity generators by incorporating public spaces such as plazas, viewing terraces, and other features that relate interior space to the public realm. These buildings may be designed as landmark structures in certain instances. It is critical that the ground level of structures containing cultural facilities be designed to provide a high level of transparency and activation along streets and public space frontages. Service locations for loading, refuse and recycling should be carefully located in less visually obvious locations, under the building with below grade access or 'back of the house' with access from a rear lane or minor street.

Cultural facilities and amenities should take advantage of key locations as activity generators by incorporating public spaces such as plazas, viewing terraces and other features that relate interior space to the public realm.



Figure 5.7.4a - Cultural facilities as activity generators



Figure 5.7.4b - Cultural facilities such as theatres



Figure 5.7.4c - Cultural buildings designed as landmark structures



Figure 5.7.4d - Seating areas adding to the public realm

5.7.5 PARKING & SERVICING STRATEGY

The built form of Lakeview Village is planned to control the impacts of parking and servicing on the public realm. Visitor parking will be located on street and accommodated in several free-standing public parking structures located between Lakeview Square and the Serson Innovation Campus. These above-grade parking structures are located to balance accessibility to easily 'intercept' visitors from the larger street network with limited visual exposure from the public realm. Parking structures will be designed to contain liner uses wrapping street frontages or provide screening of parked vehicles with either a façade treatment, graphic panels or landscaping, or some combination of the above. These 'park once' locations are strategically located to serve multiple user groups which will result in higher parking utilization for longer periods and turnover rates that generate multiple vehicles using each space during a 24-hour period.

Residential parking will consist of at-grade private garages for ground-related townhouses. For all other building types, minimal convenience surface parking for may be provided, but resident and visitor parking will be provided below-grade situated under all other building types. Driveways and ramps to below-grade parking will be strategically located to provide accessibility from a minor street or rear lane with limited visual exposure from the public realm.

PARKING

On-Street Parking

At-grade Parking (Townhouse Alleys)

Below Grade Parking

Free Standing Garage

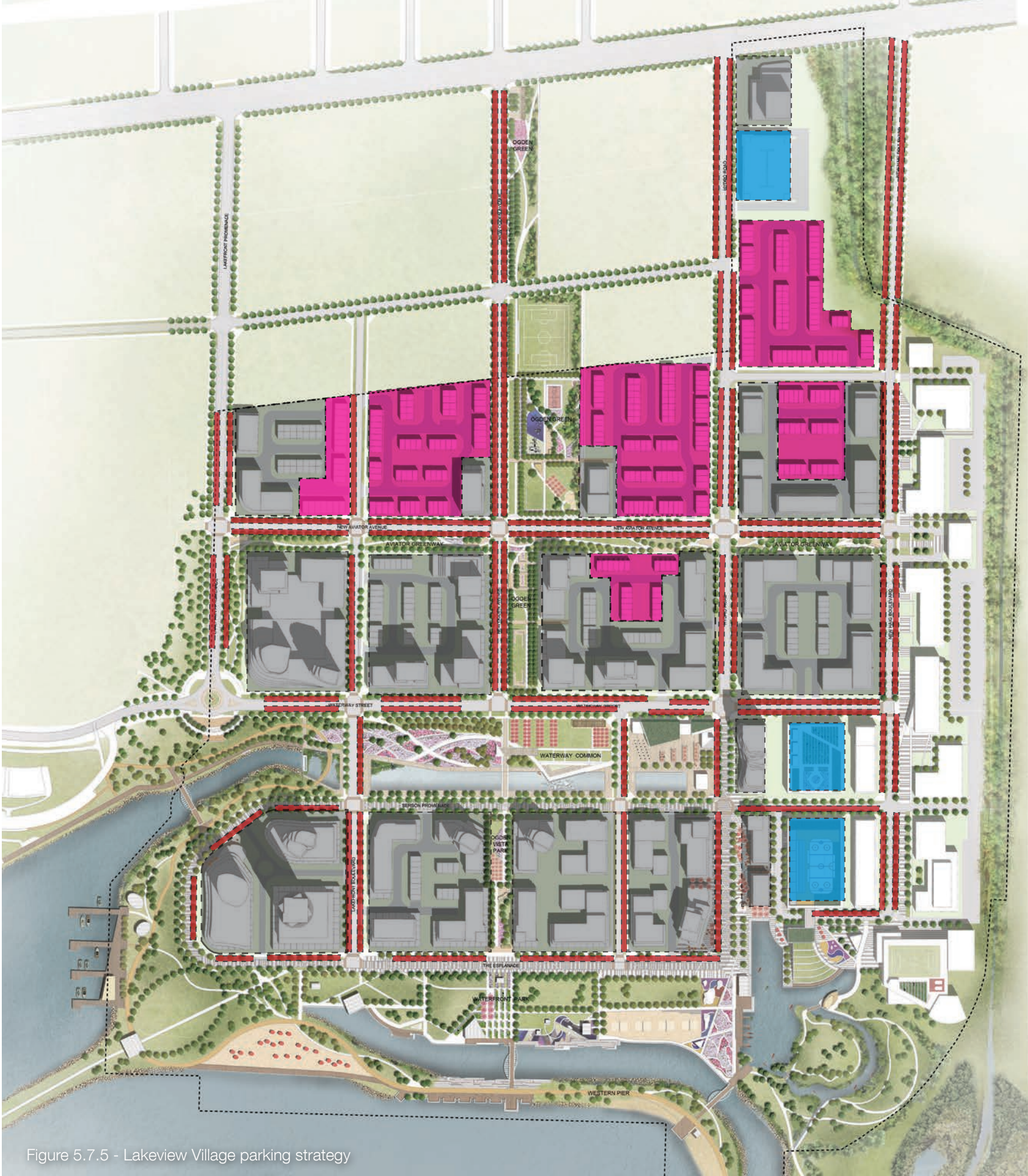


Figure 5.7.5 - Lakeview Village parking strategy

5.8 SUSTAINABILITY STRATEGY OVERVIEW

A separate stand-alone Lakeview Sustainability Strategy report has been prepared by The Municipal Infrastructure Group with input from Bicol Consulting Inc., FVB Energy, Glen Schnarr & Assoc., and McMurray Environmental.

The Lakeview Sustainability Strategy report provides a detailed commentary on the sustainable opportunities and possibilities for this project as well it looks at the financial impact of this strategy on the City of Mississauga, and it provides a roadmap to ensure implementation of the strategy is achieved. This study focuses on adding value and economics to the Lakeview Village project by targeting sustainable issues such as energy, water, environment and human well-being.

The objective of this Master Plan narrative is to provide an overview of the Sustainability Strategy goals, the sustainability protocols used, and the sustainability approaches considered. It is our hope that this narrative piques the interest of the reader such that the reader engages in reading the separate stand-alone strategy report and the dialog around it.

Our Vision for the Future

Mississauga will inspire the world as a dynamic and beautiful global city for creativity and innovation, with vibrant, safe and connected communities; where we celebrate the rich diversity of our cultures, our historic villages, Lake Ontario and the Credit River valley.
A place where people choose to be.

5.8.1 GUIDING PRINCIPLES

The following documents provided the foundation for the Lakeview Village Sustainability Strategy and provided guidance in the selection of the Lakeview Sustainability Framework structure:



Strategic Plan: Our Future Mississauga

In 2007, Mississauga Council initiated a conversation called Our Future Mississauga which became the City’s most comprehensive public engagement process to-date, connecting over 100,000 people. This conversation culminated in a report released in 2009 entitled Strategic Plan: Our Future Mississauga. In this strategic planning document, the City shared their Vision Statement which states:

This Vision Statement was supported by five Strategic Pillars for Change, namely, MOVE, BELONG, CONNECT, PROSPER, and GREEN. These five Strategic Pillars for Change have their own unique direction statement and principle, along with specific strategic goals to ensure that the City’s Vision is achieved. These Pillars are summarized in the following table:






The Strategic Pillars for Change		
 Developing a Transit-Oriented City	Direction – Our Future Mississauga is a city where people can get around without an automobile, and where transit will directly influence and shape the form of the city. Transit will be a desirable choice that connects people to destinations, and will underpin an environmentally responsible, inclusive, vibrant and successful city.	Principle – Mississauga is a city that values clean air and healthy lifestyles through the promotion of transit as a preferred, affordable and accessible choice.
 Ensuring Youth, Older Adults and New Immigrants Thrive	Direction – Our Future Mississauga is a city where people can spend their entire lives - where teenagers want to be, where young professionals choose to locate, where immigrants are welcomed and their cultures become a visible part of the city’s fabric; and where people can age in place gracefully.	Principle – Mississauga is a city that thrives on its social and cultural diversity.
 Completing our Neighbourhoods	Direction – Our Future Mississauga is a beautiful, sustainable city with safe neighbourhoods that support a strong, connected and vibrant community - a place where all can live, work and prosper. People can play as a child, walk to meet a friend, fall in love, raise a family and grow old.	Principle – Mississauga is a city that nurtures a unique quality of life within each neighbourhood, where residents value the beauty and variety of the natural environment, engage in active transportation and support a rich, healthy and prosperous social and cultural mosaic through all stages of the life cycle.
 Cultivating Creative and Innovative Businesses	Direction – Our Future Mississauga is a global hub of creative and innovative activity where talent and business thrive.	Principle – Mississauga is a city that values a strong global business future, fostering a prosperous and sustainable economy that attracts and grows talent.
 Living Green	Direction – Our Future Mississauga is a city that co-exists in harmony with its ecosystems, where natural areas are enhanced, forests and valleys are protected, the waterfront connects people to Lake Ontario, and communities are nurtured so that future generations enjoy a clean, healthy lifestyle.	Principle – Mississauga is a city that values its shared responsibility to leave a legacy of a clean and healthy natural environment.

Figure 5.8.1a - The City of Mississauga Strategic Plan’s ‘The Strategic Pillars for Change’

Within the Strategic Plan, each Strategic Pillar for Change is connected to specific action items which propels the plan forward. These action items are outlined in the City’s complementary Action Plan document that identifies targets, actions, and funding options for each Strategic Pillar for Change.

Living Green Master Plan

In 2012, the City of Mississauga released the City’s first environmental master plan. This plan was built on the 2009 Strategic Plan: Our Future Mississauga vision of a clean and healthy natural environment with healthy people, clean air and water, all in a sustainable energy-efficient urban form.

The Living Green Master Plan (LGMP) identified the actions that the City will take to address its environmental challenges and goals for the following 10-year period. These goals are:

- 1. **Implement the Strategic Plan Vision**
- 2. **Choose Priorities and Allocate Resources**
- 3. **Support Better Integration Among City Departments**
- 4. **Develop Baseline Information, Targets and Indicators to Measure Success**
- 5. **Provide Education, Public Awareness to Help Residents Live Green**
- 6. **Foster Partnerships and Collaboration**

Through this Living Green Master Plan, it was determined that of the five Strategic Pillars for Change, as identified in the Strategic Plan: Our Future Mississauga, **MOVE**, **CONNECT**, and **GREEN** are the most relevant to the LGMP.

The objectives of these three Strategic Pillars, as they relate to the LGMP are:

MOVE – Developing a transit-orient city by:

- Reducing private automobile use and developing compact mixed-use development;
- Building a reliable and convenient transit system that is frequent, clean, safe, reliable, and convenient and within walking distance of every home; and
- Adding capacity through strategic investment in transit, streets and active mobility options.

CONNECT – Completing neighbourhoods by:

- Developing walkable, connected neighbourhoods that give residents the ability to engage safely in all aspects of their everyday lives; and
- Providing transportation mobility choices.

GREEN – Living green by:

- Promoting technologies and tactics to conserve energy and water, reduce emissions and waste, improve air quality, and protect the natural environment;
- Conserving, enhancing, and connecting natural environments; and
- Promoting a green culture by changing behaviours to minimize the impact on the environment and contributing to reversing climate change.



Figure 5.8.1b - City of Mississauga Living Green Master Plan, January 2012

5.8.2 PROPOSED SUSTAINABILITY GOALS

The following are the proposed Sustainability Goals for Lakeview Village:

- To become the City of Mississauga’s first Master Planned Net Zero Energy Ready Community and strive to become a Net Zero Energy Community. In addition, this will assist in meeting the Government of Canada's goal "under the Paris Agreement, Canada has committed to reducing Greenhouse Gas (GHG) emissions by 30% below 2005 levels by 2030."
- To provide Climate Change leadership by minimizing Lakeview Village’s dependence on fossil fuels.
- To support the City of Mississauga’s Strategic Pillars for Change as outlined in the Strategic Plan: Our Future Mississauga (2009) and the City’s Living Green Master Plan (2012) by establishing a sustainability strategy which builds upon the **MOVE**, **CONNECT**, and **GREEN** pillars.
- To support the City of Mississauga’s Smart City Strategy by working closely with the City to implement key initiatives.
- To support the Region of Peel’s goal of 75% diversion of solid waste by 2034 through an efficient waste management strategy which strives towards Net Zero Waste.
- To reduce consumption and to promote reuse of water (domestic, stormwater).
- To make walking, cycling, and transit the preferred transportation option within the Lakeview Village.
- To optimize the people experience within Lakeview (relaxed experience).
 - Walkable community
 - Access to nature
 - Effective Public Realm spaces
 - Indoor and outdoor thermal comfort
 - Indoor and outdoor air quality
- To develop and incorporate sustainable features that would attract the international market, thus putting the Lakeview Village project on the global scale.

5.8.3 SUSTAINABILITY FRAMEWORK

The structure of the Lakeview Sustainability Framework will follow the **EcoDistricts Protocol** (see www.ecodistricts.org) . This protocol puts a comprehensive lens on every urban regeneration decision, drives the delivery of meaningful performance outcomes, and sets the conditions for sustainable, collective impact. Above all, the EcoDistricts Protocol is designed as a flexible performance framework, rather than a prescriptive standard, recognizing that every community has the ability and need to advance a place-based sustainability agenda. Design teams tailor the Protocol to local circumstances, set performance targets based on local conditions and aspirations, and measure progress against the Protocol’s Imperatives and Priorities.

The **EcoDistricts Protocol** provides:

- a. A tool for fostering neighbourhood and district scale sustainability.
- b. A rigorous certification standard which marks industry leadership and connects certified districts with a global peer-to-peer learning network pursuing continuous improvement.

The **EcoDistricts Protocol** is centered around three Core Elements:

3 Imperatives: Equity, Resilience, and Climate Protection

6 Priorities: Place, Prosperity, Health and Wellbeing, Connectivity, Living Infrastructure, and Resource Regeneration

3 Implementation Phases: Formulation, Roadmap, and Performance Monitoring / Reporting

3 IMPERATIVES:

Equity – Cities that embrace equity identify and acknowledge the communities most vulnerable to change. These cities experience stronger and longer-lasting growth. District Teams must ensure that their community has the opportunity to meaningfully participate, lead, and thrive.



Resilience – Resilience is the capacity of cities to function so that all people are able to withstand the shocks and stressors they encounter. District teams must address resilience with a broad approach that prepares for social, economic, and environmental shocks and stressors.



Climate Protection – Cities are responsible for the majority of global carbon dioxide emissions, the dominant greenhouse gas contributing to climate change. District teams must build a pathway towards carbon neutrality.



6 PRIORITIES:

Place – The goal is to create inclusive and vibrant communities that are complete in services and facilities, affordable, and accessible to all; places with identities rooted in local history and culture; and places where livability is the product of engaged stakeholders.



Prosperity – The goal is to support education and economic opportunities that build prosperity and accelerate innovation and business start-ups. Local resources and neighbor investors are used to enhance economic opportunities while improving social conditions and supporting locally owned and produced goods and services.



Health & Wellbeing – The goal is to nurture people’s health and happiness. Health and safety are key contributors to a sense of wellbeing among residents and workers. Fresh healthy food contributes to better health outcomes as does active living based on walkability and recreation, and reduced exposure to toxins and pollutants.



Connectivity – The goal is to build effective connections between people and places through a street network that accommodates diverse ages and abilities by using multiple travel modes and shared mobility options, and a high-quality digital network providing equitable connectivity and leveraged community data.



Living Infrastructure – The goal is to create a community which supports indigenous flora, fauna, migratory species and pollinators; conserves and replenishes fresh water, protects and restores fertile soils, and regenerates food; connects people to nature; and employs strategies to eliminate or mitigate impacts to climate and natural hazards.



Resource Regeneration – The goal is to work towards net positive energy, water, and waste where energy and water are conserved and protected, waste is minimized and reused, and greenhouse gas emissions are reduced.



3 IMPLEMENTATION PHASES:

Formation – Formation focuses on shaping the necessary leadership, collaboration, and decision-making governance to support effective action through all phases of development and implementation. Formation is the key stage in exercising a district’s commitment to procedural and structural equity in engagement and decision-making.



Roadmap – The Roadmap is a performance-based action plan that outlines a comprehensive set of projects and programs to create/improve a district’s sustainability. The Roadmap sets performance targets and implementation milestones based on the Imperatives and Priority indicators.






















Performance – Performance focuses on measuring impact and gauging a district’s progress toward strategy implementation. A biennial progress report becomes the basis for measuring impact over time.



The selection of the EcoDistricts Protocol for Lakeview Village considered the City’s Strategic Pillars for Change as outlined in the Strategic Plan: Our Future Mississauga document.

It was established that the priorities for this EcoDistricts Protocol not only closely matched the City’s Strategic Pillars, but it also provides an enhanced emphasis on health and wellbeing.

The following table summarizes the EcoDistricts Protocol and provides a correlation to the City's five Pillars for Change:

IMPERATIVES	<div><div>EQUITY</div><div>RESILIENCE</div><div>CLIMATE PROTECTION</div></div>					
PRIORITIES	<div>PLACE</div>	<div>PROSPERITY</div>	<div>HEALTH + WELLBEING</div>	<div>CONNECTIVITY</div>	<div>LIVING INFRASTRUCTURE</div>	<div>RESOURCE REGENERATION</div>
OBJECTIVE CATEGORIES	Engagement + Inclusion Culture + Identity Public Spaces Housing	Access to Opportunity Economic Development Innovation	Active Living Health Safety Food Systems	Street Network Mobility Digital Network	Natural Features Ecosystem Health Connection with Nature	Air Water Waste
IMPLEMENTATION	<div><div>FORMATION</div><div>ROADMAP</div><div>PERFORMANCE</div></div>					
OUR FUTURE MISSISSAUGA	<div><div>BELONG</div><div>PROSPER</div><div>CONNECT</div><div>GREEN</div><div>GREEN</div></div>					
	<div><div>BELONG</div><div>MOVE</div></div>					

5.8.4 SUSTAINABILITY STRATEGY OVERVIEW

Utilizing the EcoDistricts Protocol described above, a summary of the Lakeview Sustainability Strategy is outlined below:

PLACE:

The goal is to create an attractive, inclusive, and vibrant community which is complete in services and facilities, affordable, and accessible to all; a place with identities rooted in local history and culture; and a place where livability is the product of engaged stakeholders.

This will be achieved through a focus on:

- Public Spaces
 - High quality, engaging, and active public spaces.
 - Mixture of destinations.
 - Spaces which are accessible to all.
 - Connecting people to the lake, the Lakefront Trail system, and to local ecology.
- Culture and Identity
 - Historic and culturally significant places are preserved and celebrated through public art.
 - Building social capital for Lakeview's residents and workers through Community Programming.
- Diverse and Affordable Housing
 - Housing which is close to amenities that offer a complete set of daily needs.
 - A diverse mix of housing types which will attract a diverse mix of residents with varying levels of affordability.

PROSPERITY:

The goal is to support education and economic opportunities that build prosperity and accelerate innovation and business start-ups. Local resources and neighbor investors are used to enhance economic opportunities while improving social conditions and supporting locally owned and produced goods and services.

This will be achieved through a focus on:

- Economic Development
 - Creation of new employment opportunities which presently do not exist on these lands.
 - Diversity of employment within the retail, office, institutional, and restaurant land uses, providing opportunities for local business startups.
 - Attract quality high-tech employment opportunities within the Serson Innovation Corridor.
- Innovation
 - The Serson Innovation Corridor will target and attract learning / business / employment opportunities and will bring together educators, researchers, social scientists, entrepreneurs, and business experts.
 - In conjunction with Mississauga's Smart City Strategy, the Innovation Corridor is anticipated to house mobile workforce spaces called 'The Hub'. The Hubs are co-working facilities where people can learn, work, meet, and succeed. Hubs provide a safe, open, and inclusive space with access to high speed internet, community amenities, information, and support.
- Education
 - The Serson Innovation Corridor will target and attract higher education institutions with an emphasis on research and innovation.

HEALTH & WELLBEING:

The goal is to nurture people's health and happiness. Health and safety are key contributors to a sense of wellbeing among residents and workers. Fresh healthy food contributes to better health outcomes as does active living based on high quality exterior environments, walkability, and recreation.

This will be achieved through a focus on:

- Active Living
 - High walkability level through introduction of trails, parks, and public realm open spaces throughout the community. These trails will connect locations within the community as well as to the external Waterfront Trail System and the new Jim Tovey Lakeview Conservation Area ecopark currently under construction.
 - Introduction of Lakeview Square, Waterway Common, The Marina, and Inspiration Point will create destinations which will draw residents, workers, and visitors down to the water.
 - Active transportation will be encouraged through the introduction of bike lanes and pathways as well as through shared bike facilities.
- Health
 - Healthy air and soil will be achieved through the environmental remediation of the soil remaining on site from the previous coal fired hydroelectric plant activities.
 - Healthy water will be utilized in the Waterway Common water park using stormwater which has been treated to suitable levels for public contact.
 - Low Green House Gas emissions (GHG) will be encouraged and achieved through various innovative and sustainable approaches discussed in this strategy.

- Local Food Systems
 - Healthy and affordable fresh food will be available within walking distance of all residents.
 - Local food sources for both residents and local restaurants will be encouraged through the introduction of shared community gardens.



Figure 5.8.4a - High level of walkability throughout the community



Figure 5.8.4b - Shared Community Gardens

CONNECTIVITY:

The goal is to build effective connections between people and places through a street network that accommodates diverse ages and abilities by using multiple travel modes and shared mobility options, and a high-quality digital network providing equitable connectivity and leveraged community data as part of the Smart City concept.

This will be achieved through a focus on:

- Street Network
 - Street network designed to accommodate all modes of transportation with a strong emphasis on pedestrian and bicycle corridors.
 - Street network designed to accommodate people with a diverse range of age and ability.
 - Connecting residents and visitors to the lake, the Lakefront Trail system, the Jim Tovey Lakeview Conservation Area and to local ecology.
- Mobility
 - Shared mobility options are to be available through shared car and shared bicycle facilities.
 - Public transit is to be extended into the heart of Lakeview Village along a route which is within walking distance of all residents and employees.
 - An electric shuttle bus service will be available to assist residents and workers in accessing the higher order public transit on Lakeshore Road until such time when public transit is extended into the community.

- Smart City Technologies
 - Attract global talent and investors into the development of 'smart sustainable future districts' to spur economic prosperity and quality of life for its citizens.
 - Broadband / Fibre Optic Network / WiFi to be provided throughout the community within the public realm spaces and roadways.
 - In conjunction with Mississauga's Smart City Strategy, the Serson Innovation Corridor is anticipated to house indoor mobile workforce space called 'The Hub'. The Hubs are co-working facilities where people can learn, work, meet and succeed. Hubs provide an open and inclusive space with access to high speed internet, community amenities, information and support. Hubs are a safe space to make connections and to learn.
 - In conjunction with Mississauga's Smart City Strategy, the Waterway Common and Lakeview Square districts are to house Outdoor 'Mini-Hubs' workspaces.



Figure 5.8.4c - Outdoor 'Mini-Hubs'

LIVING INFRASTRUCTURE:

The goal is to create a community which supports indigenous flora, fauna, migratory species and pollinators; conserves and replenishes fresh water, protects and restores fertile soils, and regenerates food; connects people to nature; and employs strategies to eliminate or mitigate impacts to climate and natural hazards.

This will be achieved through a focus on:

- Ecosystem Health
 - Contaminated land, from previous industrial land use, to be remediated to residential quality standards.
 - The impact of development on stormwater will be managed through various Low Impact Development (LID) measures.
 - Air quality and greenhouse gas emissions will be significantly reduced, from previous land use, through the implementation of various measures identified in this strategy.
- Connection to Nature
 - Trails, parks, and public realm open spaces throughout the community will connect people to both internal and external natural habitats including Lake Ontario, the Waterfront Trail System and the new Jim Tovey Lakeview Conservation Area ecopark currently under construction.
 - Recreational water activities such as paddle boats, kayaking, and canoeing will not only draw people to the water but to nature within and around the water features.

- Natural Features
 - The quality and function of habitats within the site will be drastically enhanced through the conversion of the site from a largely barren industrial site to a multi-use development with an integrated natural heritage system.
 - Serson Creek will be realigned and rehabilitated to provide an enhanced natural feature.

RESOURCE REGENERATION:

The goal is to work towards net zero energy, water, and waste where energy and water are conserved and protected, waste is minimized and reused, and greenhouse gas emissions are reduced significantly.

This will be achieved through a focus on:

- Energy
 - Net zero energy ready building construction methodology will be mandated and incorporated throughout Lakeview providing both active and passive measures to reduce energy demand (EUI Target 100kwh / m2).
 - Recycle all energy and resource waste as much as possible before generating energy.
 - Renewable power and thermal energy are produced and distributed on site. Enough renewable energy will be planned so this development will eventually become a Net Zero Energy Community with zero GHG emissions.

- Water
 - Stormwater capture combined with sustainable storm water treatment prior to discharge to Serson Creek and Lake Ontario.
 - Stormwater re-use to be encouraged through rooftop capture and cistern storage.
 - Various Low Impact Development (LID) measures will be incorporated including:
 - Greenroofs, urban rain gardens, bioswales, tree cells, permeable pavements, etc.
- Waste
 - Efficient waste sorting and collection program to be incorporated to encourage waste reduction and diversion from landfill.
 - Existing on site concrete slab foundations to be removed and repurposed as fill material in support of the creation of the new Jim Tovey Lakeview Conservation Area ecopark currently under construction.
 - Incorporation of a vacuum waste system to be analyzed and considered for Lakeview Village.

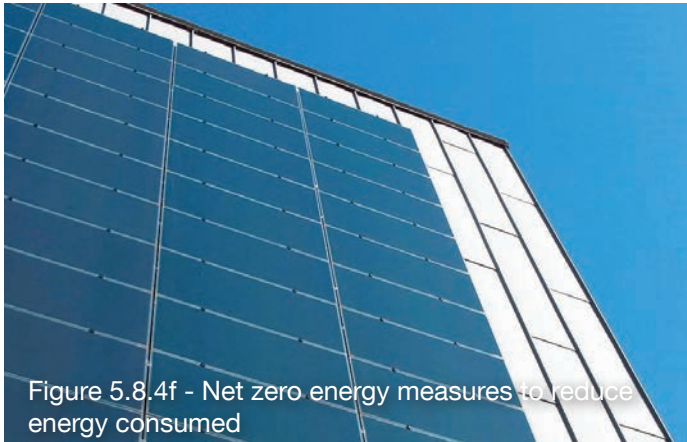




Figure 5.8 - Illustrative representation of Lakeview Village's sustainable features

5.8.5 STORMWATER MANAGEMENT
INTEGRATION AND LINKAGES

The 2014 Master Plan established the principle of ‘green’ and ‘blue’ fingers traversing the community to provide ecological and water linkages, respectively. The current development master plan maintains the green fingers by the arrangement of linear park spaces, with water features integrated into right-of-way corridors facilitating the principle of the blue fingers.

Section 6.2 provides discussion on the overall servicing strategy for the development master plan, which includes the proposed stormwater management strategy. Within this context, key public elements are proposed to integrate water management infrastructure with other community infrastructure and functionality. In particular, stormwater management infrastructure is proposed to be included within rights-of-way wherever possible to provide conveyance and control in accordance with established requirements, within a form that suits the specific local and urban design vision. Similarly, two significant water features are proposed which showcase water management and treatment principles in a manner that is accessible and functional for public use.

The range of low impact development features under consideration within the community include green roofs, permeable pavement, bioswales, bioretention, rainwater harvesting, tree pits with soil cells, and bioretention planters, curb-cuts or bump-outs. The images below provide examples of each of these LID approaches.

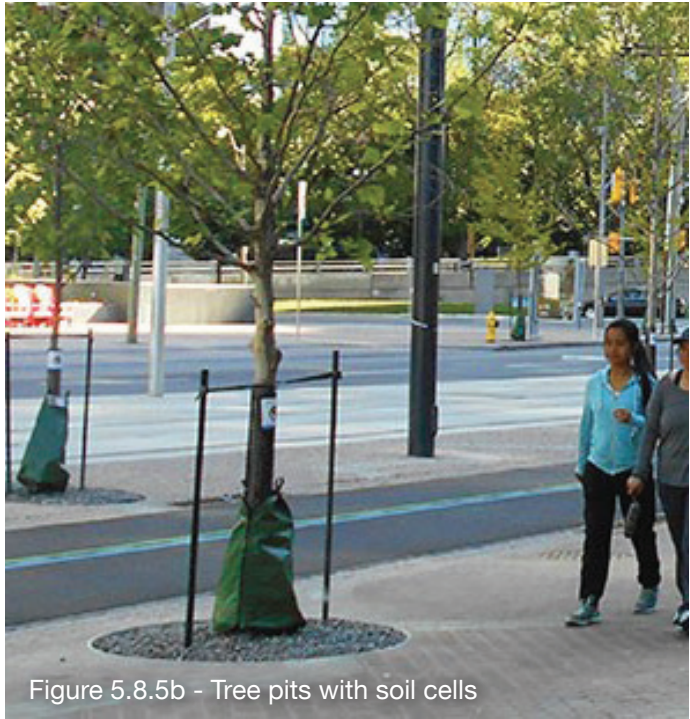


Figure 5.8.5b - Tree pits with soil cells



Figure 5.8.5a - Tree pits with soil cells



Figure 5.8.5d - Bioretention as a LID feature



Figure 5.8.5c - Bioretention planters



Figure 5.8.5e - Bioretention planters

CENTRAL WATER FEATURES

Stormwater is proposed to be reused in two facilities: the water feature within Waterway Common and the recreational pond within Inspiration Park. The vision for both facilities is quite different in terms of water quality treatment required. The Waterway Common water feature is intended to facilitate direct human contact, while the recreational pond will have indirect contact with people. The type of health risk and public perception of water quality for each feature will also differ because of the type of uses for each facility. The Waterway Common water feature is intended for use as a splash pad with children running through and playing with the water, and the public expectation of the water quality for this type of facility will be similar to a public pool. Conversely, the recreational pond is intended for paddle boating and other non-motorized water crafts. The public expectation of this body of water will be similar to the lake.



Figure 5.8.5f - Stormwater reuse water feature

Waterway Common Water Feature

The water feature is located in the middle of the subject lands within the Waterway Common. Runoff from the community will be directed to a subsurface storage facility. Depending on feasibility, the existing concrete structure under Waterway Common can be modified and used to provide this function. This structure was part of the outflow system from the previous coal plant and could be modified as a settling tank before pumping to a water treatment facility. Since the proposed feature will have direct contact with people, appropriate water quality standards are anticipated to be applied. Consultation and approval from Region, City, and MECP will be required for the water feature.

The vision for the water feature is to maintain a continuity of the water from the lake into the Lakeview community. The water feature will sit above the lake level but will flow into the lake. In addition, a shallow water depth will be maintained within the water feature to produce a mirror like appearance and a continuation of the water from the lake. This feature will serve multiple purposes depending on the weather. In the summer, the public can splash around and play within this space and in the winter at the most upstream end of the water feature it can be turned into an outdoor skating rink.



Figure 5.8.5g - Multi purpose water feature



Figure 5.8.5i - Water feature for all ages



Figure 5.8.5h - Multi seasonal water feature



Figure 5.8.5j - Splash pad water feature for children

Recreation Pond

The recreational pond is located within Inspiration Park at the southeast corner of the subject lands. It is also located adjacent to the future parkland to the east. The pond will provide a transition between urban and parkland by providing docks and steps along the western edge and will transition into a wetland space along the east to blend into the parkland space. The pond will provide opportunities for the public to paddle boat, and to try other non-motorized water crafts such as kayaks. Since these activities are usually associated with activities on the lake, the water quality expectation from the public will be similar to lake water quality. Additional treatment is not anticipated to be required.

Runoff from the community will be conveyed to the pond and treated on route via oil-grit separators and various LIDs. The resulting water will have at least 80% TSS removal, with additional polishing of runoff occurring through the pond before discharge to Lake Ontario. Ensuring requisite treatment occurs upstream of the pond will minimize intrusive maintenance requirements within the pond.



Figure 5.8.5k - Conceptual Plan for Inspiration Point's recreation pond

5.9 STREETS

The community will be structured by a fine grain street pattern with a well-ordered hierarchy that will appropriately integrate transit connections and various densities and buildings types, support logical walking and cycling linkages throughout the community and achieve efficient block development.

The character of the streets will vary depending on function and adjacent land use types. Minimum street right-of-way widths are reinforced and alternative road standards considered to ensure the best response to balancing pedestrian, cycling, transit, and vehicular use with a scale conducive to the adjacent land use types, functions, and architectural massing. Influences from shared streets or 'woonerfs' are encouraged where appropriate to reinforce pedestrian comfort, provide unique streetscape opportunities and achieve a reduction in right-of-way widths. Innovative LID features will also be considered within street right-of-way's as a key component of a broader, comprehensive sustainability strategy.

5.9.1 STREET HIERARCHY & ALIGNMENT

A well-defined and logically connected hierarchy of streets forms the main structure of the Lakeview Village. It will provide for the safe and convenient movement of pedestrians, cyclists and vehicles, serve as common space for social interaction and help establish the character and visible impression of the community.

Designed as a fine-grained street pattern, the street network established for Lakeview Village responds to the existing surrounding road network, the site's topography, waters edge constraints and existing uses found along the community's edges. The proposed road layout is intended to facilitate convenient and efficient movement and circulation, support accessibility and transit ridership, and promote safe pedestrian and cycling oriented lifestyles.

A particular structural emphasis will be connections to the waterfront, ensuring linkages and view corridors to the water's edge are reinforced through street orientation and connecting opportunities.

The streets are designed to minimize block lengths for easier navigation and walkability, and to create terminating views, vistas and other focal points to achieve an attractive public realm.

The proposed street network consists of collector roads, minor collector roads, local streets and character streets (pedestrian priority), in addition to the existing Lakeshore Road East arterial road.

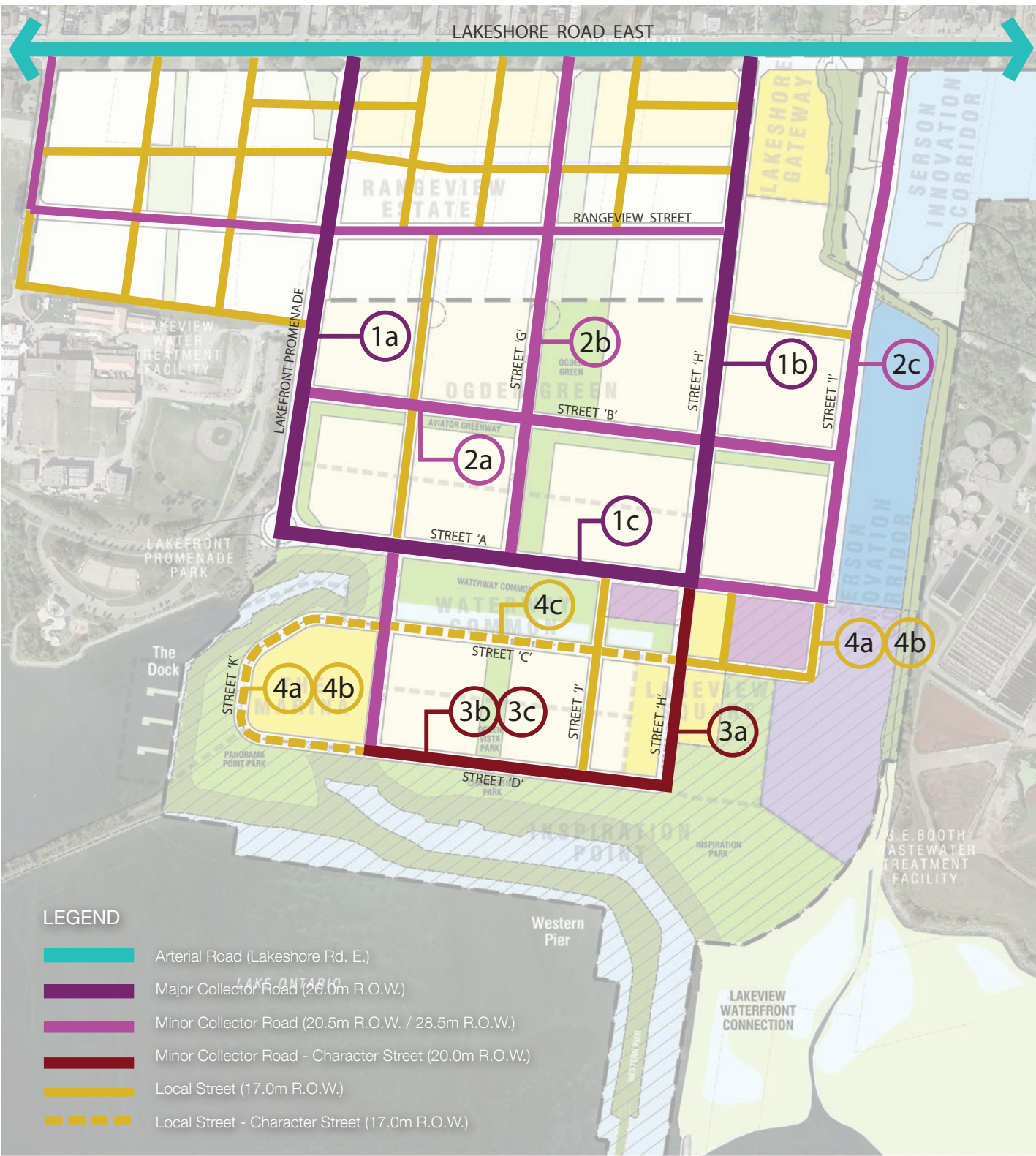


Figure 5.9.1 - Preliminary street hierarchy

5.9.2 FINE-GRAIN STREETS & NEIGHBOURHOOD STRUCTURE

Lakeview Village’s interconnected street/block layout in a modified grid pattern is designed to facilitate movement and permeability throughout the pedestrian-scaled village. With a primary emphasis on pedestrian comfort, smaller block lengths and convenient direct pedestrian linkages reinforce a walkable, urban village environment. Neighbourhood amenities such as parks, transit stops, and greenways are located within a reasonable walking distance, which corresponds with an approximate five-minute (or 400-metre) walking radius. With an emphasis on permeability for pedestrians, the modified grid layout reduces travel distance, and increases the opportunity for a variety of experiences.

VIEW CORRIDORS & PERMEABILITY

An extensive network of parks and open space provides a range of opportunities for attractive views both within Lakeview Village and towards the lake. Important views and viewsheds, combined with linkages to the green corridors will enhance permeability through the village and connectivity between its open spaces and parks system. Throughout the master planning process, these potential view opportunities have influenced the configuration of land uses, building siting, and layout of the street network.

Emphasis has been placed on locating open space amenities along potential view corridors and architectural built form is also located, oriented, and designed to maintain and emphasize views.

A major north-south view corridor has been allocated through Lakeview Village, starting at Lakeshore Road East, running through Rangeview Estates, Ogden Green, Waterway District and Lakeview Inlet, terminating at Inspiration Point. The park system has been strategically aligned with this corridor connecting a series of linear parks parallel to the street and several significant parks, including Ogden Green, Waterway Common, and Ogden Vista Park

SITE ACCESS & PRIVATE DRIVEWAYS

Vehicular, cycling and pedestrian access to Lakeview Village will occur primarily from the major collector roads running south from Lakeshore Road East. Secondary access will be from the minor collector roads that terminate at Waterway Common.

5.9.3 STREET TYPOLOGIES (RIGHT-OF-WAYS)

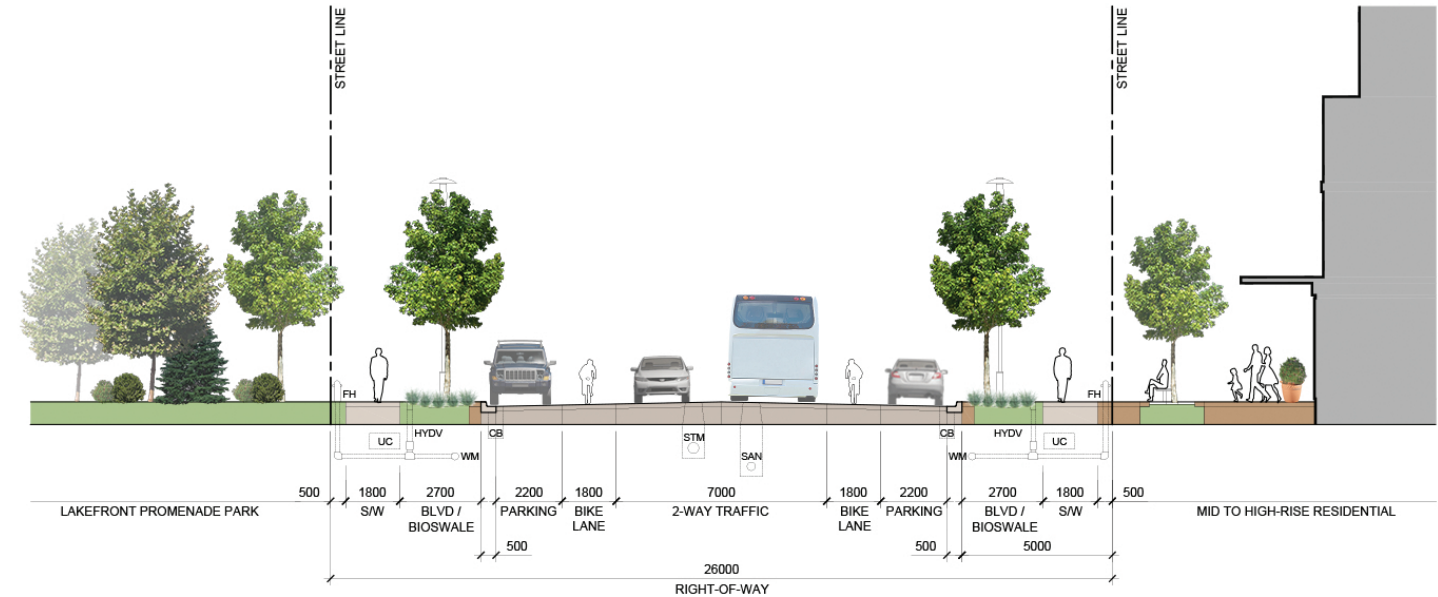
As indicated, the street typologies proposed for Lakeview Village are represented within four general categories:

- 1. **Collector Streets**
 - Lakefront Promenade
 - Hydro Road
 - Waterway Street
- 2. **Minor Collector Streets**
 - New Aviator Avenue
 - New Ogden Avenue
 - New Haig Boulevard
- 3. **Minor Collector Special Character Streets**
 - The Square
 - The Esplanade
- 4. **Local Streets**
- 5. **Local Special Character Streets**
- 6. **Future Street Networks (Lakeshore Neighbourhood and Rangeview Estates)**

A. MAJOR COLLECTOR ROADS

Major collector roads provide important connections between Lakeview Village districts and community functions, such as parks, recreation centres, and other facilities. They largely define the community structure, serve as the primary inter-district circulation routes, and accommodate transit.

The major collector road right-of-way width is 26.0 metres. Streetscape character varies according to land uses, which range from high-rise residential, mid-rise residential, rear lane townhomes, Lakefront Promenade Park, Waterway Common, and mixed-use mid-rise buildings.



Lakefront Promenade (1a)

Lakefront Promenade will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, bike lanes, street furniture as appropriate to adjacent uses, and urban street tree conditions in bioswale boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on the development side of the street;
 - Bike lanes in each direction; and
 - Boulevards with bioswales.

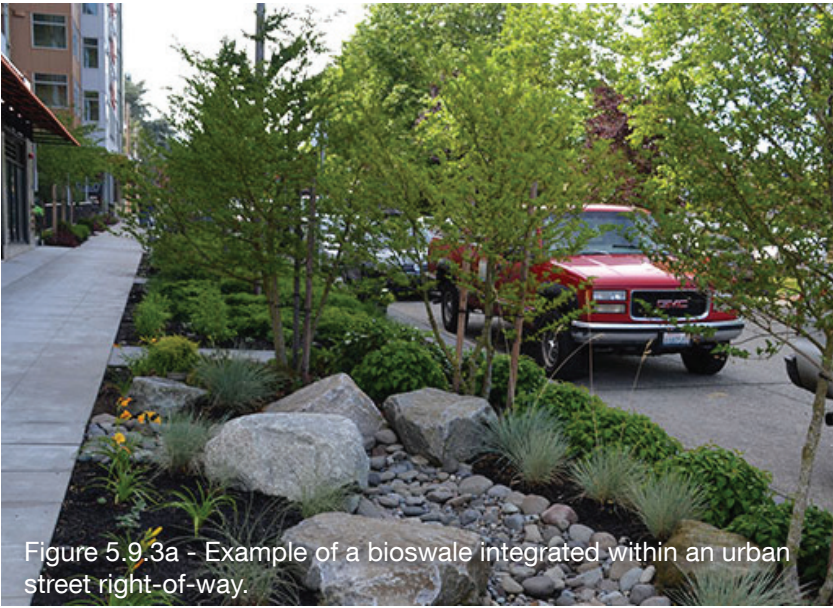
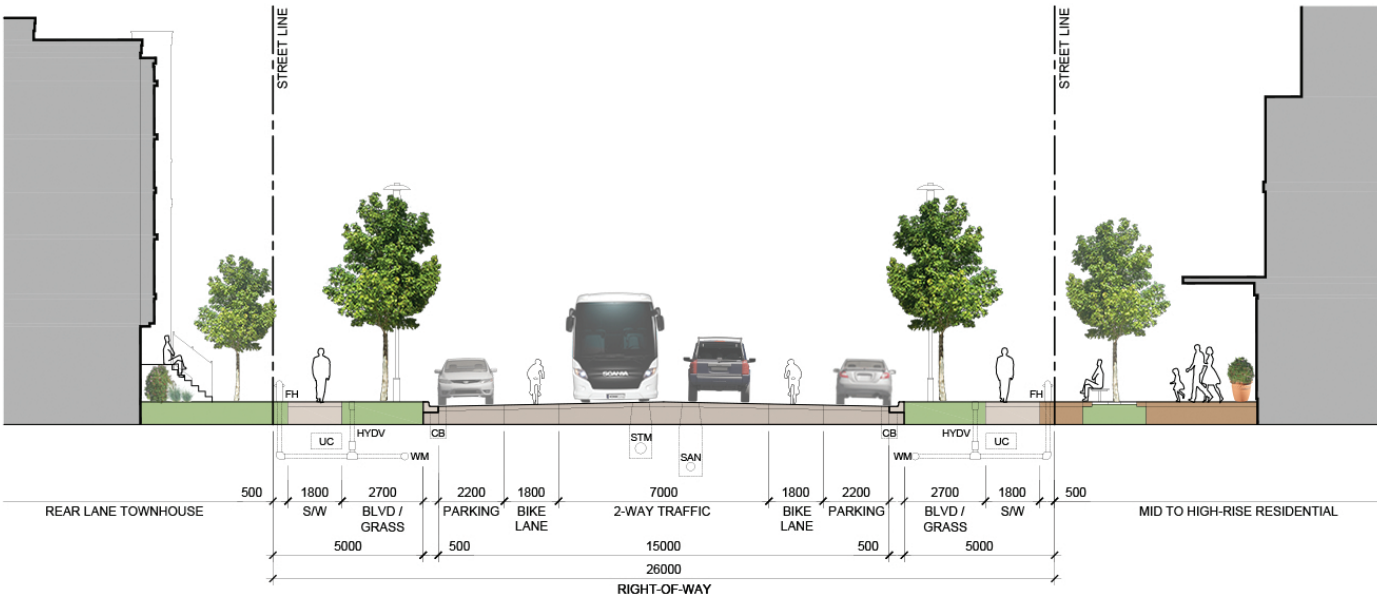


Figure 5.9.3a - Example of a bioswale integrated within an urban street right-of-way.

Hydro Road (1b)

Hydro Road will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, bike lanes, street furniture as appropriate to adjacent uses, and urban street tree conditions in grass boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on both sides of the street;
 - Bike lanes in each direction; and
 - Grass boulevards.



Waterway Street (1c)

Waterway Street will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, bike lanes, street furniture as appropriate to adjacent uses, and urban street tree conditions in raised curb stormwater management planter boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on both sides of the street;
 - Bike lanes in each direction; and
 - Boulevards with raised curb stormwater management planters.

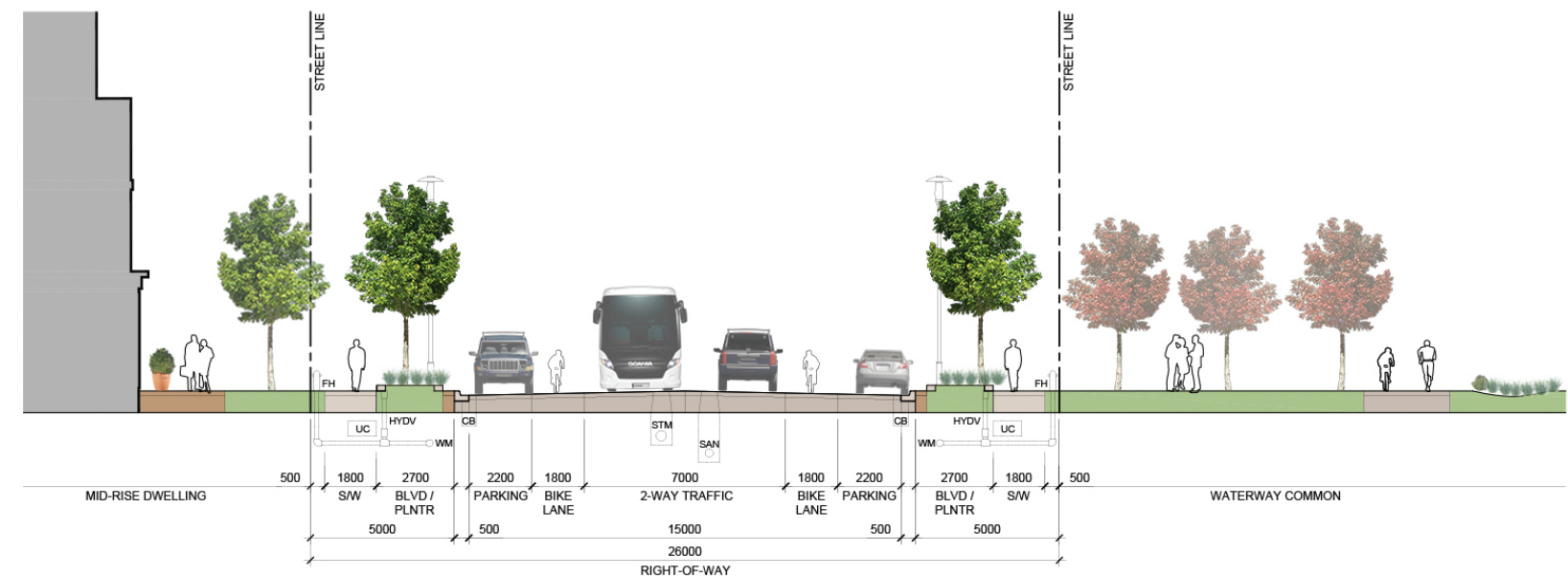


Figure 5.9.3b - Example of a stormwater planter integrated with the streetscape design in an urban setting



B. MINOR COLLECTOR ROADS

Minor collector roads also provide important connections between Lakeview Village districts. They further define the community structure and serve as the primary circulation routes.

The minor collector road right-of-way width is 20.0 metres. Streetscape character varies according to land uses, which range from townhomes, Aviator Greenway, Ogden Green, and Serson Campus.

New Aviator Avenue (2a)

New Aviator Avenue will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, street furniture as appropriate to adjacent uses, and urban street tree conditions in grass boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on both sides of the street; and
 - A multi-use path within adjacent Aviator Greenway.

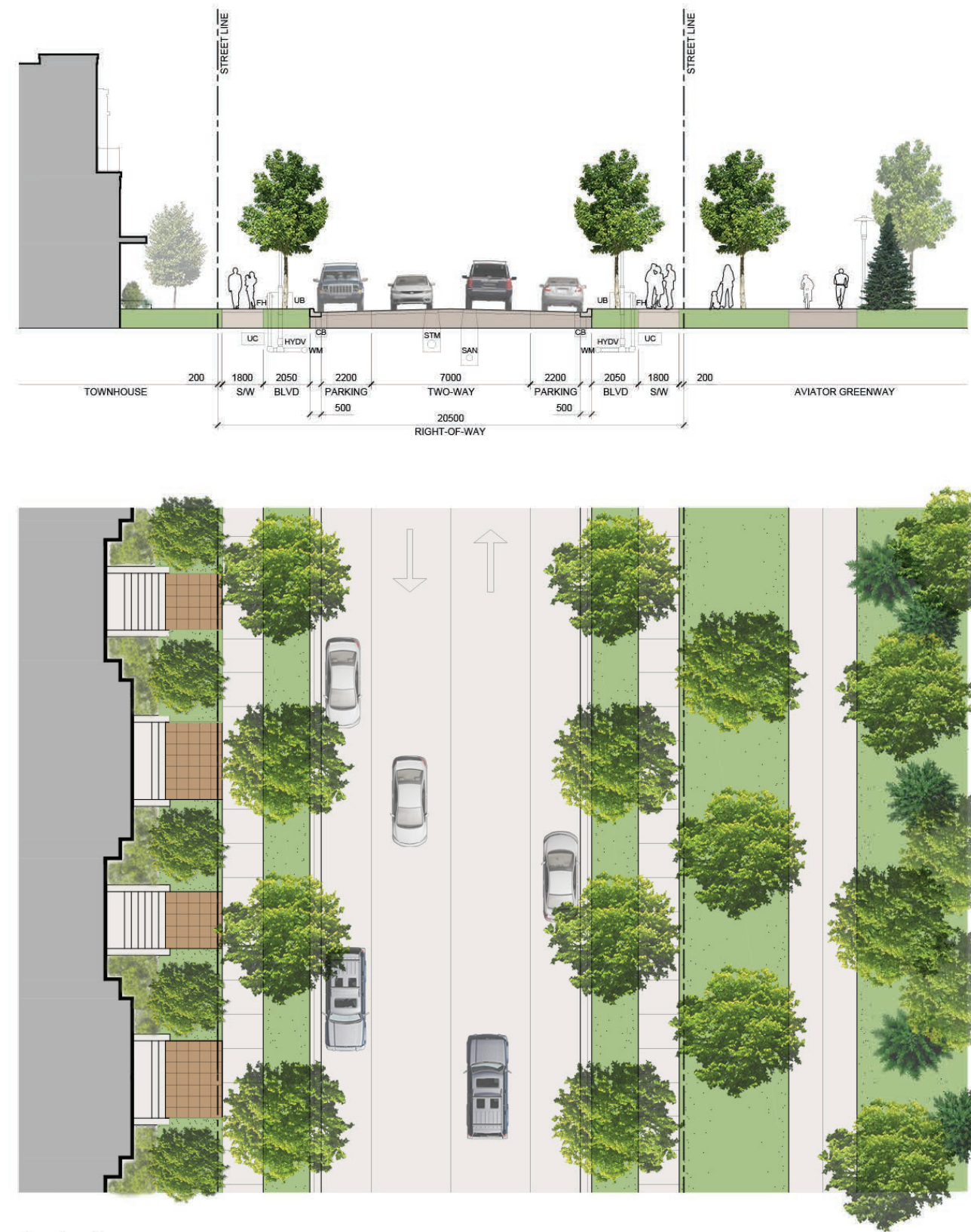


Figure 5.9.3c - Example of multi-use path situated within a park adjacent to a street right-of-way

New Ogden Avenue (2b)

New Ogden Avenue will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, street furniture as appropriate to adjacent uses, and urban street tree conditions in grass boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on both sides of the street; and
 - A multi-use path within adjacent Ogden Green.

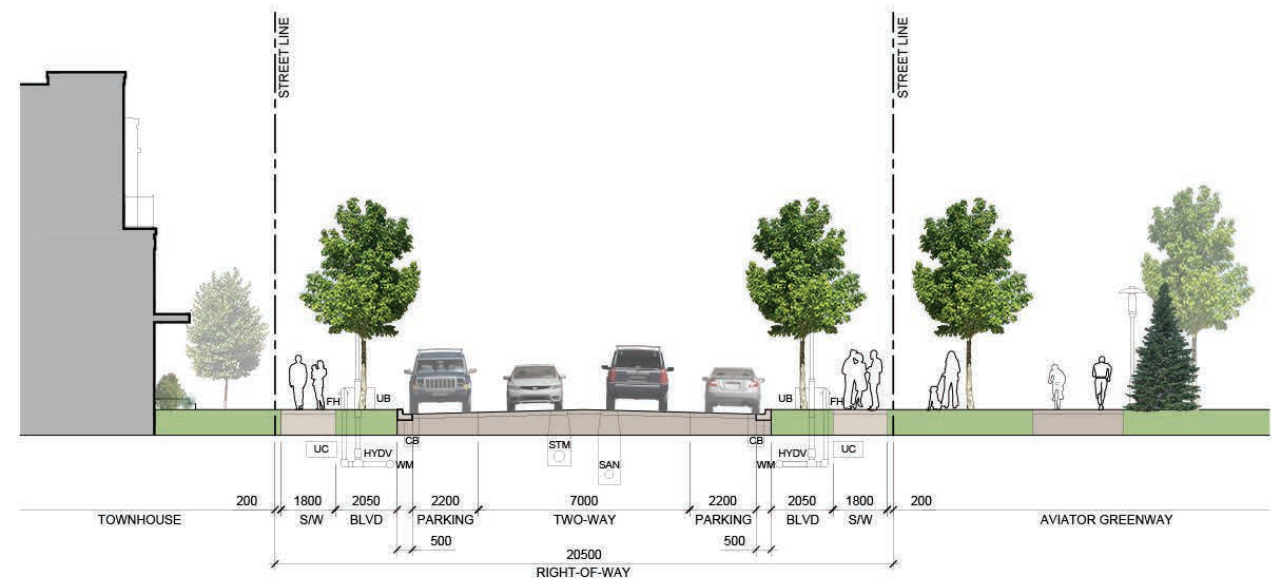


Figure 5.9.3d - Example of townhouses with a reduced setback and strong relationship with the street

New Haig Boulevard (2c)

New Haig Boulevard will incorporate urban streetscape treatments characterized by enhanced paving, sidewalks, and urban street tree conditions in grass boulevards.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on both sides of the street; and
 - Street furniture and landscaping within adjacent Serson Campus.

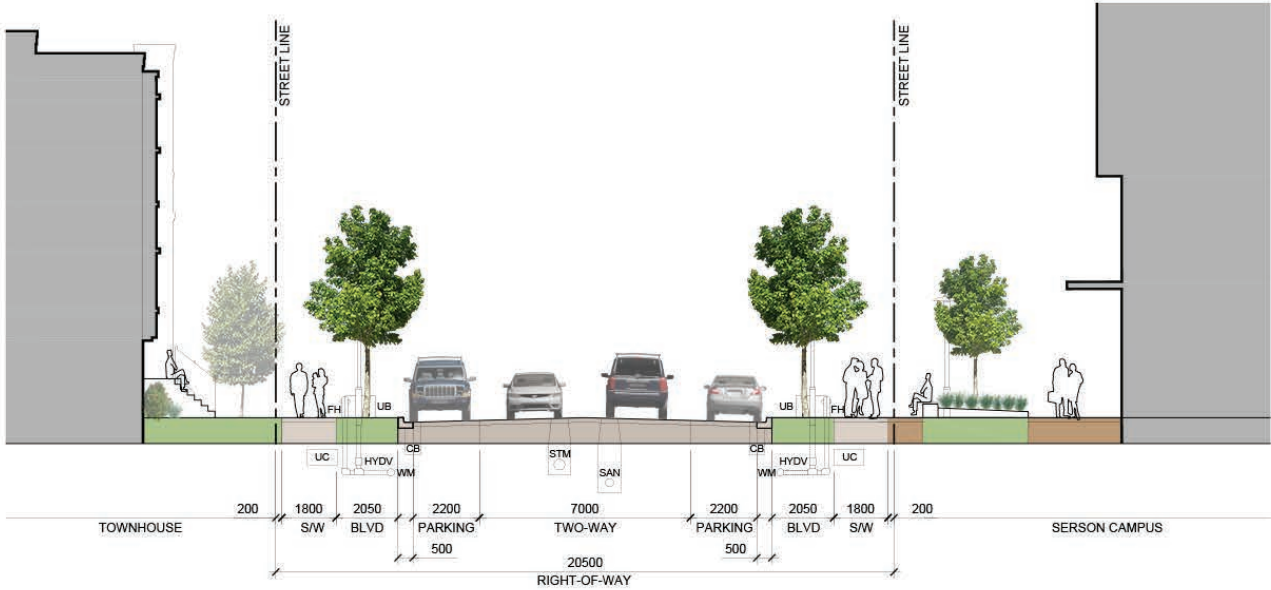
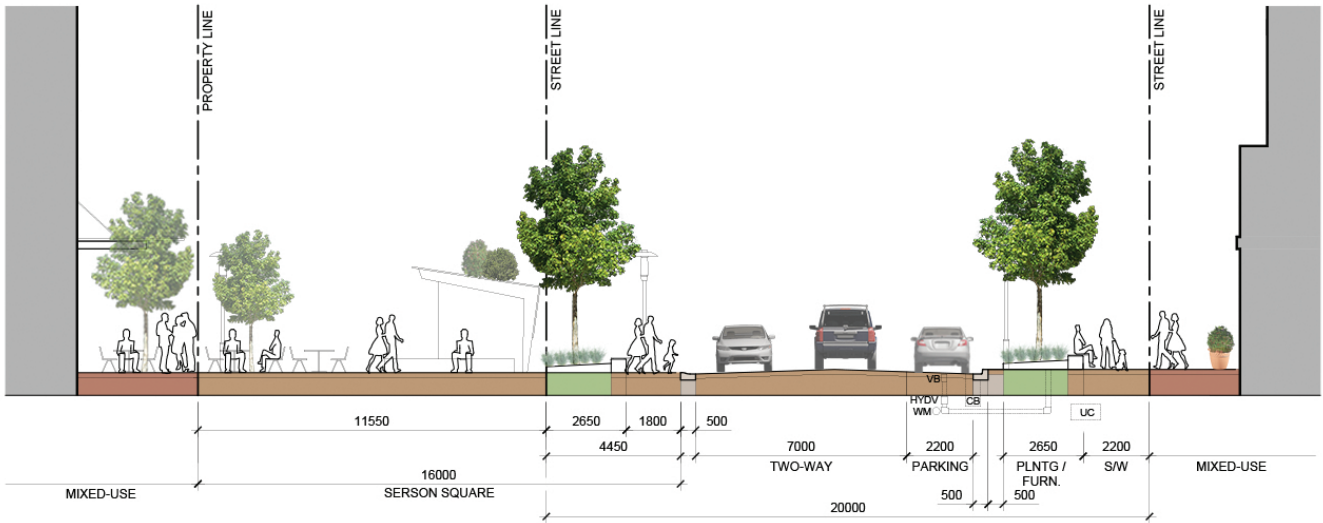


Figure 5.9.3e - Example of urban street tree conditions and a boulevard treatment

C. MINOR COLLECTOR - SPECIAL CHARACTER STREET

Minor collector special character streets serve as vital functions within the Lakeview Village community. They define the community structure and provide circulation adjacent to important public spaces within the community.

The minor collector special character street’s right-of-way width is 20.0 metres. As character streets, they will be distinguished by streetscape treatments that support the adjacent land uses and built form types found along their edges. Streetscape character varies according to land uses, which range from mid-rise residential, mixed-use buildings, Lakeview Square, and Waterfront Park



Special Character A (3a)

Special Character A will be integrated with Lakeview Square and will incorporate urban streetscape treatments characterized by a shared street with pedestrian priority, a sidewalk on one side, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on one side of the street;
 - One lane in each direction with pedestrian priority;
 - On-street parking on the development side of the street; and
 - Street furniture and landscaping within adjacent Lakeview Square.

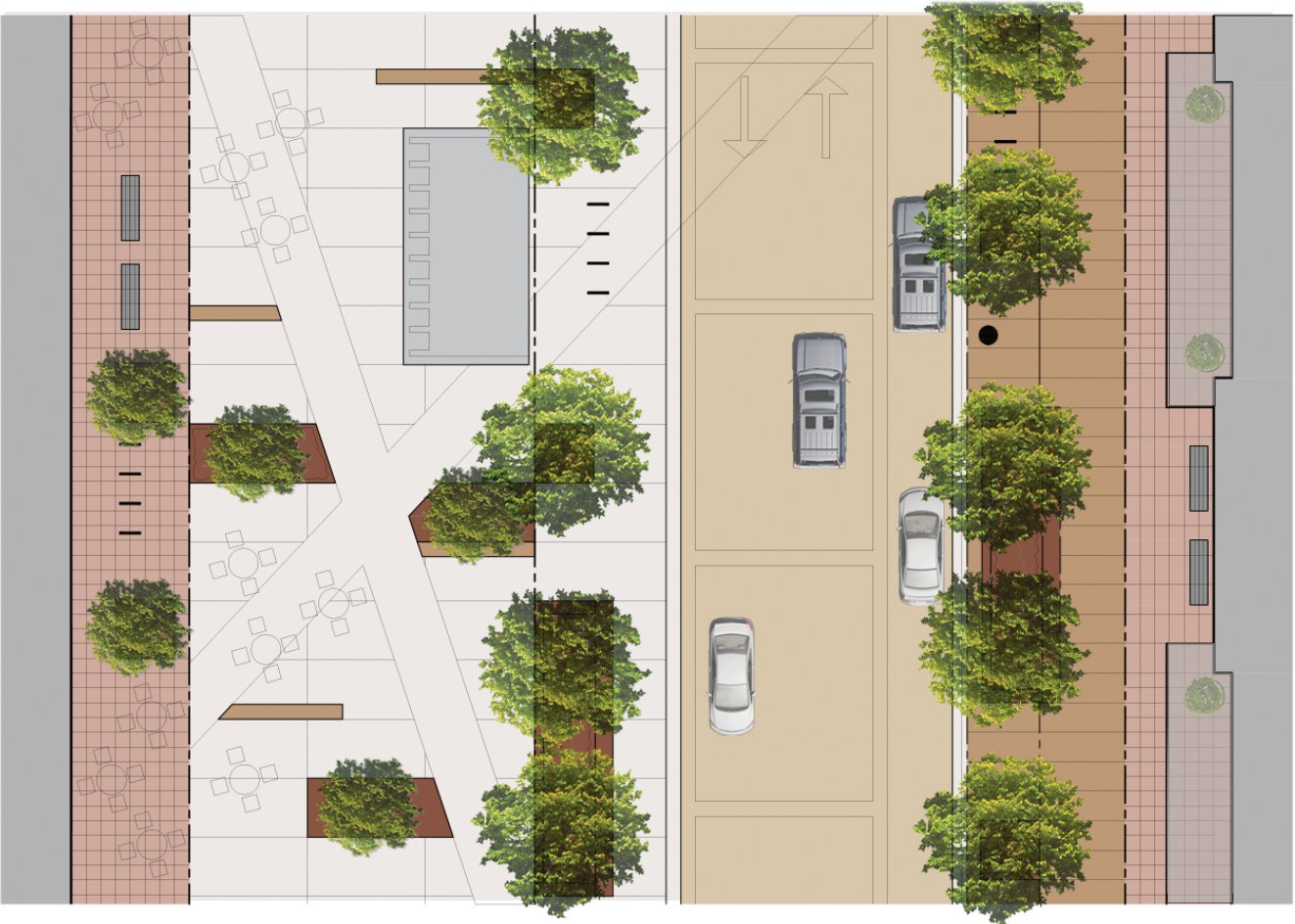


Figure 5.9.3f - Example of decorative paving extending from plaza space through the adjacent street

The Esplanade (3b)

The Esplanade will incorporate urban streetscape treatments characterized by a shared street with pedestrian priority, a sidewalk on both sides, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction with pedestrian priority;
 - Layby on-street parking on the development side of the street; and
 - A multi-use path within adjacent Waterfront Park.

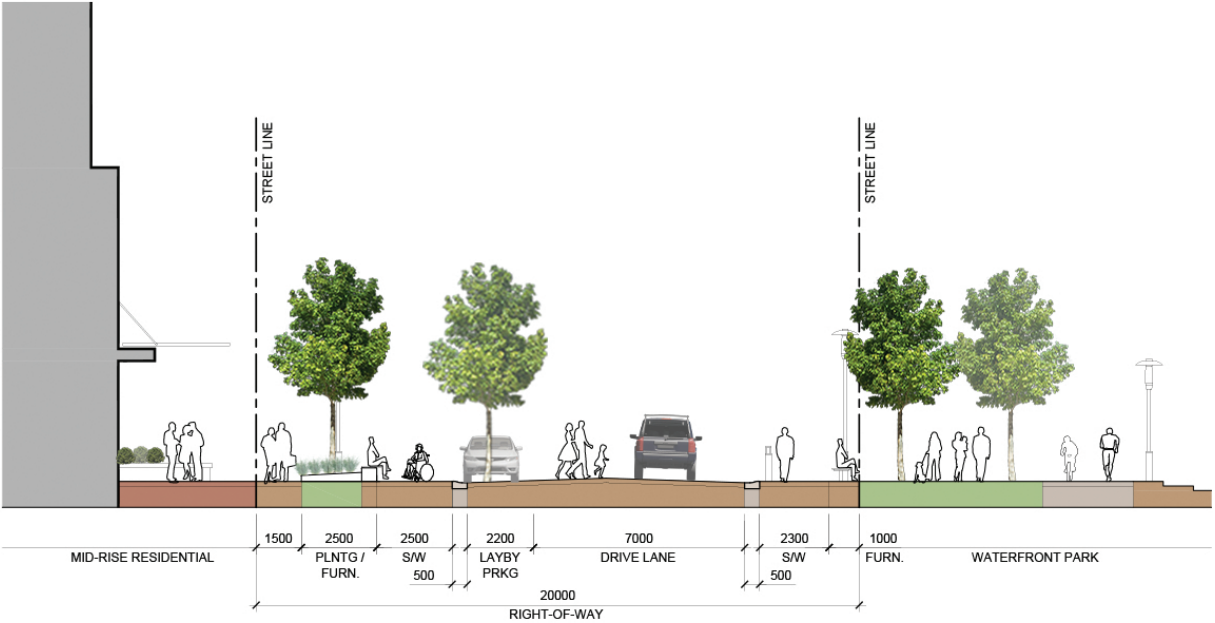


Figure 5.9.3g - Example of shared street or woonerf at the interface of a waterfront park (Malmo, Sweden)

The Esplanade (3c)

The Esplanade will incorporate urban streetscape treatments characterized by a shared street with pedestrian priority, a sidewalk on both sides of the street, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction with pedestrian priority;
 - Layby on-street parking on the development side of the street; and
 - A multi-use path within adjacent Waterfront Park.

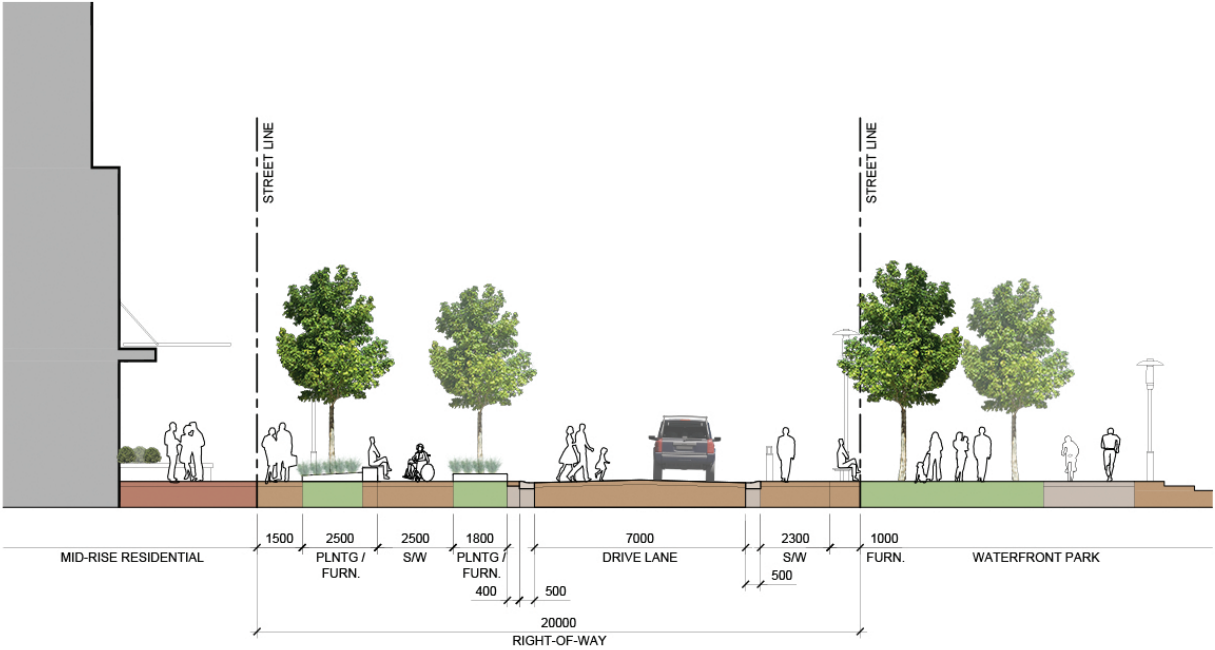


Figure 5.9.3h - Example of a street with decorative paving that reinforces the pedestrian priority

D. LOCAL STREETS

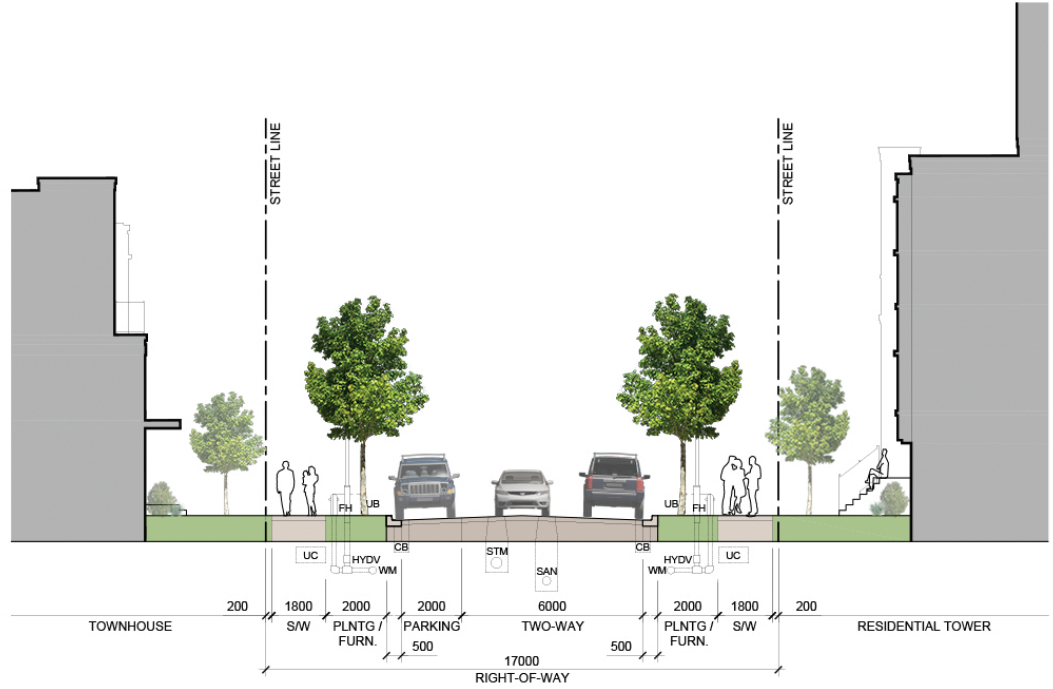
Local roads serve various districts within Lakeview Village and are intended to provide a comfortable pedestrian experience with relatively low levels of local vehicular traffic. Their character varies according to adjacent built form, which include townhouses, mid-rise residential, mixed-use buildings, and Waterway Common park.

The local street’s right-of-way width is 17.0 metres.

Local Road A (4a)

Local Road A will incorporate urban streetscape treatments characterized by adjacent land uses, a sidewalk on both sides of the street, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction; and
 - On-street parking on one side of the street.



Local Road B (4b)

Local Road B will incorporate urban streetscape treatments characterized by adjacent land uses, a sidewalk on both sides of the street, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction; and
 - On-street parking on one side of the street.

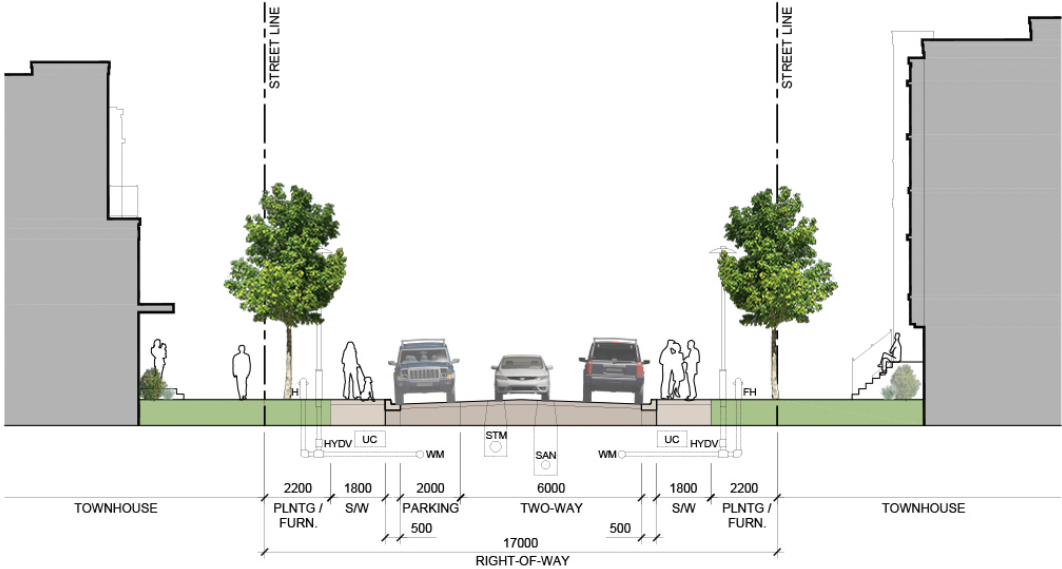


Figure 5.9.3i - Example of a local street with sidewalks situated along the curb

Serson Promenade (4c)

Serson Promenade will incorporate urban streetscape treatments characterized by adjacent land uses, a sidewalk on both sides of the street, urban street tree conditions and plantings, and street furniture.

- Typical roadway cross-sections include:
 - Sidewalks on both sides of the street;
 - One lane in each direction;
 - On-street parking on the development side of the street; and
 - A boardwalk within adjacent Waterway Common.

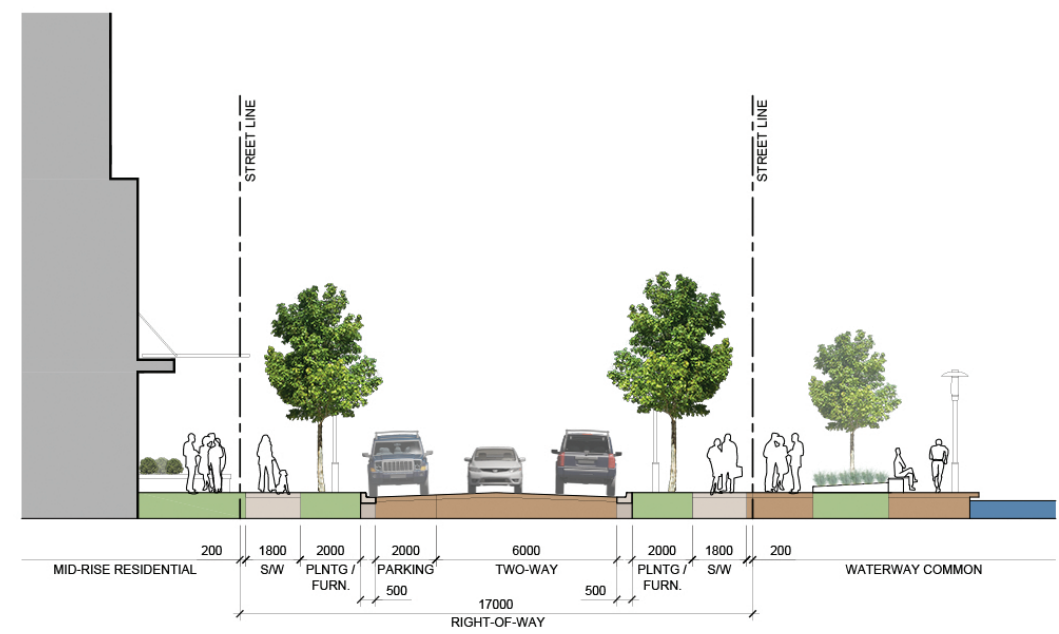


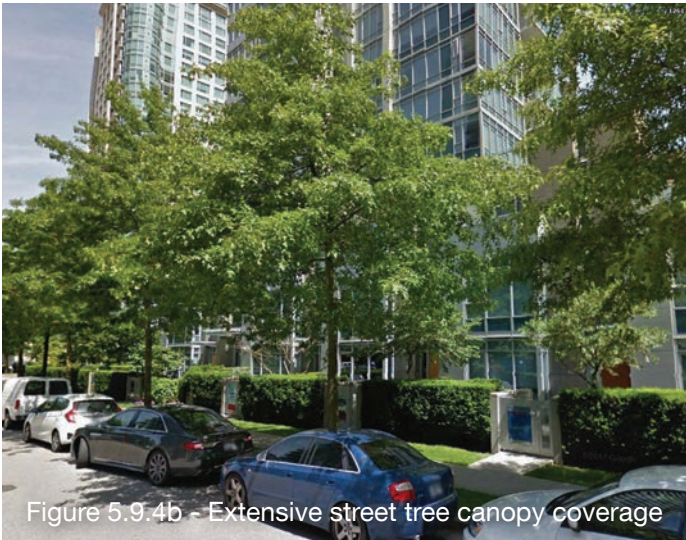
Figure 5.9.3j - Conceptual image of a streetscape interface condition along Waterway Common

5.9.4 STREETSCAPE ELEMENTS

Harmoniously designed streetscapes will contribute to the identity of Lakeview Village and each of its districts and neighbourhoods. A unique and inviting public realm experience for residents and visitors can be created through carefully considered combination of streetscape features. Elements such as lighting, site furniture, fencing reinforces the pedestrian priority and enhances the character of the village, and unique streetscape features at key areas can help distinguish each district. The proposed streetscape treatment will be appropriate to the street designation, and ensure the safety, comfort and accessibility of pedestrians, cyclists and motorists.

The design of successful urban villages hold in common several key characteristics, including:

- Providing visual interest in both the built form and streetscape at a pedestrian scale.
- Sidewalk design that balances the needs of pedestrian movement and amenities such as benches and streetscape elements including street trees and planting beds.
- Strongly defined pedestrian crossings, which may include the use of decorative paving such as coloured paving or stamped or pressed concrete.
- Lay-by parking in strategic areas that provides convenient access and supports the retail, commercial, and service uses.
- Sidewalks on both sides of the street to provide choice for pedestrians, and promote walkability.
- Lighting design that ensures safe pedestrian and vehicular circulation, as well as helping to define the character of the Village.
- Sidewalk widths are maximized along storefronts with consideration to the provision of an appropriate canopy or arcade treatment for pedestrian weather protection.
- The street trees and planting strategy will provide a balance between enhancing the vegetated environment through ecological sustainability and establishing an urban tree canopy, and meeting aesthetic requirements.



5.10 TRANSIT STRATEGY

Lakeview Village is part of the broader Lakeview Community Node and will accommodate a variety of housing, employment, cultural activities, and an extensive open space network that provides access to Lake Ontario. The land adjacent to Lakeshore Road East is being planned as a medium-to-high density corridor to be served with higher order transit (see Lakeshore Connecting Communities study by the City of Mississauga), supported by future local transit routes that will ultimately extend into the Lakeview site to support this transit-oriented community.

Local services provide the greatest opportunity to drive ridership at the neighbourhood level. The future Lakeview transit route will be very similar to many of the existing local routes, operating at similar levels of service and headways. Transit riders will use this route to access local destinations, such as schools or shopping, and as connections to the corridor routes and facilities for longer trips like the GO Stations (Port Credit & Long Branch), TTC, and the future Hurontario-Main LRT.

The long-term local transit plan utilizes the planned major collector road network in the north-south and east-west directions. These roads will form part of a circuitous route accessing Lakeshore Road between Lakefront Promenade and New Haig Boulevard (north-south), with an internal east-west connection via Waterway Street.

All residential, commercial, and institutional development will be located less than 225 metres from the internal Transit system which will define the planned transit service route. Proposed bus stops will be implemented at a maximum spacing of 250 metres along the transit route, to make travel by transit as attractive as possible to new residents and employers.

To ensure new residents, employees, and visitors generated and attracted to the community can rely upon, and become familiar with, attractive and competitive transit service at the onset of development, it is recommended that the City of Mississauga Transit Authority investigate the opportunity to modify or add bus routes into the site at first occupancies. Alternatively, LCPL proposes private shuttle service between the initial phases of the Lakeview Village to connect to Lakeshore Road (and potentially other destinations such as, Port Credit and Long Branch GO Stations, Square One, etc.) until transit demand satisfies the City’s threshold to provide public transit routes through the site.

The actual route of initial transit service will be governed by the overall system services in operation at the time, occupation percentage of the development, and practical integration of the new route.

As mentioned, the City of Mississauga is carrying out the Lakeshore Connecting Communities study and is considering Bus Rapid Transit (BRT) along Lakeshore Road through the Lakeview community. The study provides an opportunity to develop improvements along the major arterial and other transit supportive corridors so that people living or working in Lakeview Village have an attractive and competitive alternative to private auto travel.

The proposed infrastructure improvements envision exclusive transit lanes on Lakeshore Road between Southdown Road and the east City limit, and Royal Windsor Drive between the west City limit and Southdown Road. Within proximity of the Lakeview community, express buses in dedicated median lanes is preferred from East Avenue to Etobicoke Creek. The Lakeshore Connecting Communities study identifies potential far-side curb BRT stations at Lakefront Promenade and Haig Boulevard on each side of Lakeshore Road.

Lakeview Village plans to continue to work with partners from other levels of government, including Metrolinx and the private sector, to explore sustainable transportation solutions. The area’s proximity to existing and expanded all day two-way GO Rail transit service, proposed higher order transit along Lakeshore Road East and future enhanced transit into the site will provide increased levels of service and significant person-carrying capacity enhancements.

As a fully realized community, transit and active transportation will not only be viable alternatives to private vehicular use but will help shape and support the travel habits of residents, employees and visitors to the future Lakeview Village area. Enhanced transit, a fine grain road network, and the use of Transportation Demand Management measures will support transit and active transportation that will reduce reliance on private auto travel, reduce congestion, and mitigate greenhouse gas emissions.



Figure 5.10a - Lakeshore Road East is planned as a Bus Rapid Transit (BRT) route with direct links to the nearby GO Stations



Figure 5.10b - Emerging technologies bring convenient transit options to supplement mass transit networks



Figure 5.10c - Built in flexibility to seamlessly adapt to future transit options



Figure 5.10d - Preliminary master plan concept showing planned BRT transit and potential route extension into Lakeview Village

5.11 COMMUNITY BENEFITS

There are applicable statutory regulations with regards to Section 37 of the Planning Act, contributions for development. The Lakeview Waterfront Major Node MOP policies include the following:

13.4.11.18 *In exchange for increased height above 15 storeys or density permissions above the targets established in Table 1: Distribution of Housing and Unit Targets by Precinct, a community benefits contribution pursuant to Section 37 of the Planning Act will be required.*

13.4.11.19 *Notwithstanding section 13.4.11.18, in the Waterway District Area in exchange for increased height above 25 storeys, a community benefits contribution pursuant to Section 37 of the Planning Act will be required.*

13.4.11.21 *Eligible community benefits may include, but are not limited to:*

- *sustainable measures above and beyond those typically provided through development charges and parkland dedication contributions;*
- *community facility / infrastructure;*
- *affordable housing; and*
- *public art.*



Figure 5.11a - Existing Lakefront Promenade Park situated to the west of Lakeview Village

The intent of the LCPL plan is that Section 37 community benefits would result from the proposed increases in height and units as indicated in the formal development application submission materials.

While the opportunities for community benefits are noted here, the actual review of City requirements, and possible implementation opportunities for both on and off-site features will be reviewed, negotiated and implemented through the processing of future development applications.

LCPL envisions the following direct use of s.37 contributions:

OFF - SITE:

- Small Arms Building
- Lakeshore Road East (transit stations, streetscape, intersection improvements beyond TIS/phasing requirements)
- Lakefront Promenade Park, Douglas Kennedy Park, Marie Curtis Park
- Improvements / odour mitigation measures for Wastewater Treatment Plant

ON - SITE:

- Public Park enhancements (Waterway Common, Lakeview Square, Waterfront Parks/Western Pier)
- Increased sustainable / green requirements implemented throughout the site that are beyond the prescribed limits as set in the City’s requirements.
- Comm. Services satellite works yard: (temporary or permanent options)
- Affordable Housing (land/buildings/JV opportunities)
- Historical preservation / monuments, digital monuments through geocache tour
 - Public Art
 - Serson Creek Corridor enhancement
 - Streetscape improvements



Figure 5.11b - Re-use of infrastructure as public art in parks

5.12 PUBLIC ART

Lakeview Village’s distinct history provides a set of unique opportunities for high quality and harmonious public art that supports the Master Plan’s goal to recognize and celebrate the site’s past.

Varying forms of public art may range from iconic public art that contributes to a sense of place to temporary art installations such as lighting displays as part of special events. Public art could potentially relate to memorable elements including the Four Sisters, or themes such as Lakeview’s aviation past, the site’s significance during the Second World War or the local Indigenous history. With an extensive public open space system there are several prominent locations to provide connections to this valued history with meaningful public art. These locations may include key public spaces such as Waterway Common, Lakeview Square, and various locations along the waterfront shoreline.

The public art developed and selected specific to this site should be inclusive and approachable to a broad demographic and variety of cultures. It should also consider the context of the location, geographical setting, and the architecture of the built form. Public art competitions may be an effective method to increase exposure for Lakeview Village, provide opportunities for public involvement, and contribute to a sense of excitement for the development of this new community.

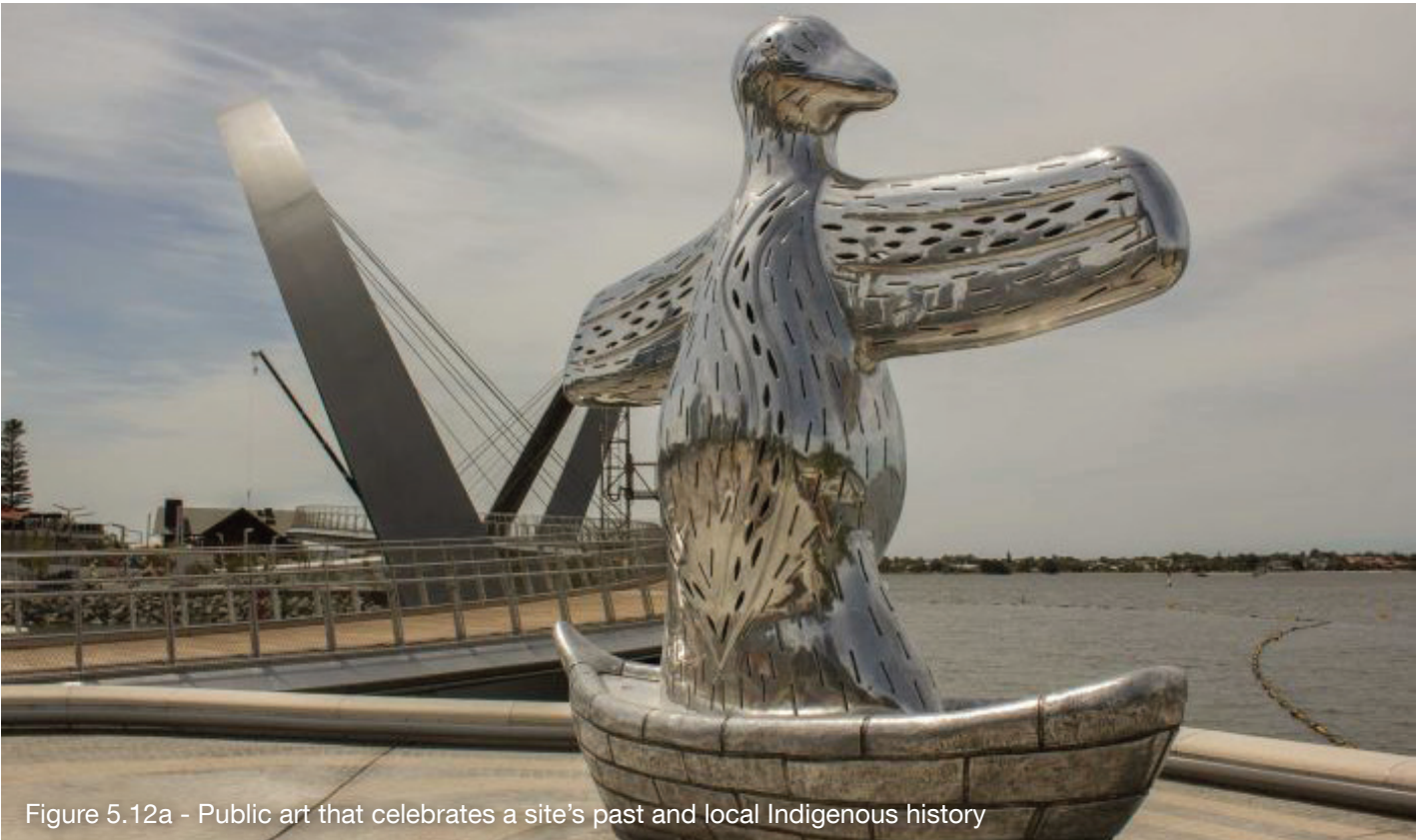


Figure 5.12a - Public art that celebrates a site’s past and local Indigenous history



Figure 5.12b - Interactive public art at all hours of the day



Figure 5.12c - Public art that could potentially relate to the iconic ‘Four Sisters’

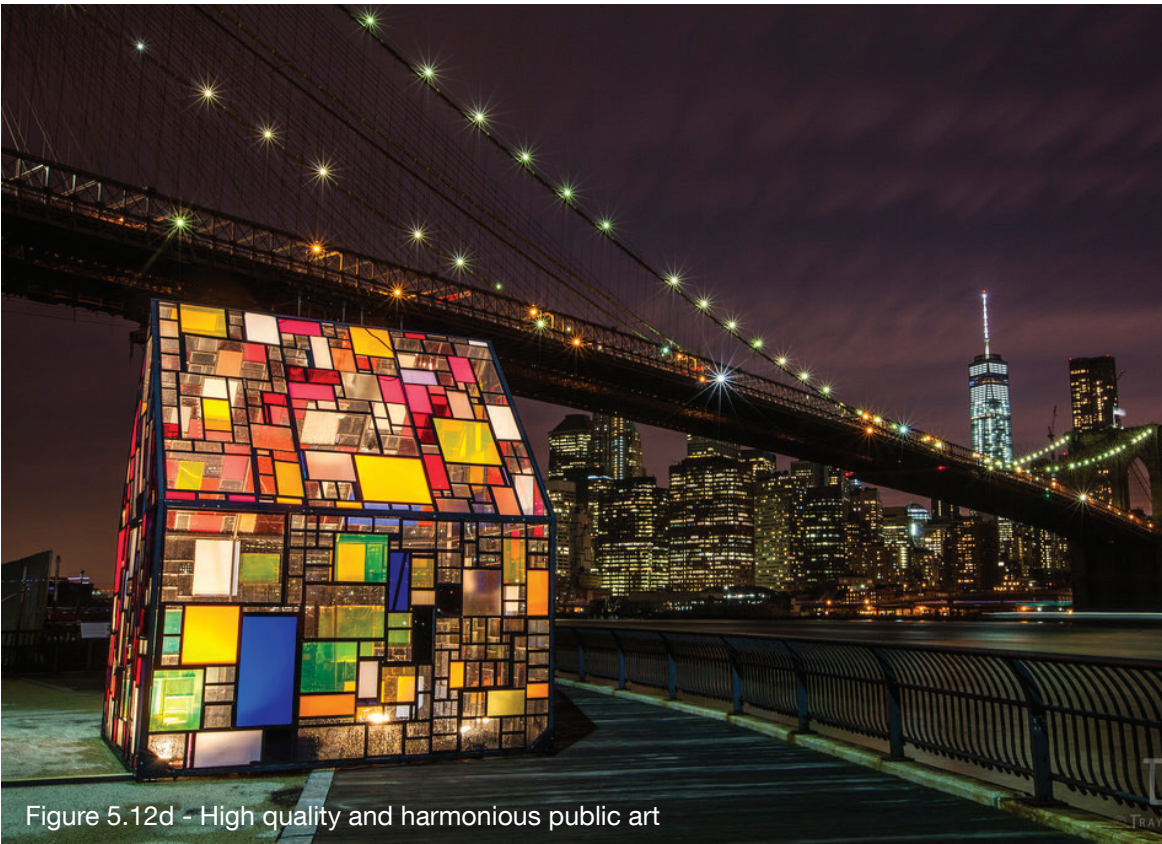


Figure 5.12d - High quality and harmonious public art



Figure 5.13f - Preliminary concept illustrating Lakeview Square, the recreation pond, amphitheatre, and Serson Innovation Corridor

5.13 CULTURAL AMENITIES AND PROGRAMMING

A cultural hub at the water’s edge is an important opportunity for the City of Mississauga and Lakeview Village.

5.13.1 CULTURAL PROGRAM

Cultural programming at Lakeview Village includes countless events, festivities, and venues that incorporate and encourage arts, culture, and expression. Cultural uses may include a museum or gallery, indoor and outdoor festival spaces, public art, public and community gardens, or a creative industry incubator. Flexible venues and outdoor space can also be used for seasonal artisan markets, farmers markets and food festivals, children’s arts and cultural programming, multicultural festivals and programs, or live performances. The plan currently shows approximately 55,000 square feet of cultural uses, in the form of a garden building, a market building, and a cultural tourism destination. These uses are placeholders that illustrate the vision and intent of the cultural users, however, will evolve based on City of Mississauga priorities, as well as funding and partnership opportunities.



Figure 5.13a - Cultural destinations and flexible outdoor venues



Figure 5.13b - Cultural destination for families



Figure 5.13c - Flexible venues and outdoor spaces



Figure 5.13d - Spaces that encourage arts, culture, and expression



Figure 5.13e - Pop-up festivals and events

5.13.2 RETAIL PROGRAM

Retail uses in Lakeview will be concentrated in Lakeview Square, with more traditional neighbourhood convenience retail located along Lakeshore Road, some seasonal uses along the inlet and the pier, as well as a potential signature waterfront restaurant in the Marina District. The total retail program for Lakeview, not including seasonal shops, is approximately 125,000 square feet.



Figure 5.13f - Seasonal outdoor patio opportunities

Lakeview Square Retail

Lakeview Square will be a new destination for unique waterfront dining and activity in Mississauga – a place to visit, play, and explore. With a mix of independent / local restaurants and active storefronts, Lakeview Square will bring the energy of Lake Ontario into Lakeview Village, complementing the beauty of the waterfront, the vibrancy of its cultural destinations, and the activity of Waterway Common.

Retail in the square will include a collection of best-in-class, independent, non-national operators. Targeted uses may include: restaurants with patios, a bakery, café, or microbrewery, specialty retail stores, an ice cream shop, a sports outfitter, and many more uses to create a true sense of place for the Lakeview community. The exact retail mix will be determined through a specialty leasing process that ensures high quality, local and regional tenants. The plan currently shows approximately 56,000 square feet of retail in Lakeview Square.



Figure 5.13g - Traditional neighbourhood conveniences



Figure 5.13h - Neighbourhood market



Figure 5.13i - A signature waterfront restaurant



Figure 5.13j - Water's edge patio



Figure 5.13k - Marina district restaurant

Lakeshore Gateway Retail

Retail uses along Lakeshore Road will likely be a mix of traditional neighbourhood conveniences, such as a grocery store, a bank, or a pharmacy. These uses will serve the Lakeview Village population, as well as the surrounding community and those driving along Lakeshore Road. The plan currently shows approximately 58,000 square feet of retail in this district.

Retail at the Water's Edge

Retail at, or near, the water's edge, provides visitors and residents with variety of opportunities to experience Lake Ontario. Retail options considered include two signature waterfront restaurants in the Marina District, as well as small seasonal shops along the inlet or at the base of the pier. Seasonal kiosks such as food and beverage offerings, unique treats and snacks, a café and retail options such as a boat and bike rental shop or a kite shop will also help to activate the waterfront.

The plan currently shows two 5,000 square foot Marina District restaurants. Tenants operating businesses in Lakeview Square will likely operate many of the seasonal shops along the water's edge to give each business operation the opportunity to be fully vested with the project and to provide the best overall customer experience.

Lakeview Village Retail Program - Overall Tenant Mix

Lakeview Square

Unit	Size (SF)	Category	Use
A1	14,918	Civic/Cultural	Market Building/ Interactive Museum
Total Sq. Ft. Building A		14,918	
B1	6,200	Retail	Bicycle Outfitter
B2	2,100	Neighbourhood Services	Lakeview Info Centre
B3	6,000	Retail	Sport Outfitter
B4	3,000	Food & Beverage	Bakery
Total Sq. Ft. Building B		17,300	
C1	2,023	Food & Beverage	Rental Kiosk/Café
Total Sq. Ft. Building C		2,023	
D1	3,100	Food & Beverage	Aviation Diner
D2	1,800	Retail	Specialty Children's Boutique
D3	1,600	Retail	Pet Accessories
D4	1,500	Neighbourhood Services	Salon
D5	1,600	Neighbourhood Services	Convenience Store
D6	1,300	Retail	Olive Oils & Vinegars
D7	1,800	Retail	Home Accessories & Gifts
Total Sq. Ft. Building D		12,700	
E1	7,200	Food & Beverage	Microbrewery
E2	800	Food & Beverage	Ice Cream
Total Sq. Ft. Building E		8,000	
F1	1,500	Food & Beverage	Vegan Grab & Go
F2	1,300	Retail	Active Retail (Running, Yoga)
F3	2,000	Neighbourhood Services	Specialty Fitness
F4	2,000	Neighbourhood Services	Spinning Studio/ Juice Bar
F5	900	Food & Beverage	Treat Shop (Cupcakes, Donuts, etc.)
F6	3,300	Food & Beverage	BBQ Smokehouse
Total Sq. Ft. Building F		11,000	
G1	2,413	Food & Beverage	Italian Trattoria
G2	2,413	Food & Beverage	Lake-to-Table Bistro
Total Sq. Ft. Building G		4,826	
H1	10,893	Neighbourhood Services	Co-Working/ Fitness
H2	1,200	Food & Beverage	Sake/ Noodle Bar
H3	1,600	Food & Beverage	Speakeasy/Cocktail Bar
Total Sq. Ft. Building G		13,693	
TOTAL Lakeview Square (Sq. Ft.):		84,460	
TOTAL Retail/F&B/Services/Office (Sq. Ft.):		56,549	

Inspiration Point & The Marina

Unit	Size (SF)	Category	Use
M1	5,000	Food & Beverage	The Marina Restaurant
IP1	500	Food & Beverage	Tacos
IP2	400	Food & Beverage	Wine Bar/Tapas/Oyster Bar
IP3	200	Food & Beverage	Ice Cream
IP4	300	Food & Beverage	Hot Dogs/Burgers
IP5	500	Food & Beverage	Fish & Chips
IP6	1,500	Retail	Bikes & Boats
IP7	500	Retail	Kite Shop
IP8	250	Food & Beverage	Coffee
Total Sq. Ft. Building M & IP		9,150	
TOTAL Inspiration Point (Sq. Ft.)		4,150	
TOTAL Marina District (Sq. Ft.)		5,000	

Lakeshore Gateway

Unit	Size (SF)	Category	Use
L1	35,000	Traditional Neighborhood Conveniences	Grocery Store
L2	10,000	Traditional Neighborhood Conveniences	LCBO
L3	10,000	Traditional Neighborhood Conveniences	Pharmacy
L4	3,000	Traditional Neighborhood Conveniences	Bank
Total Sq. Ft. Building L		58,000	
TOTAL Lakeshore Gateway (Sq. Ft.)		58,000	
TOTAL PROJECT PROGRAM (Sq. Ft.):		151,610	
Retail	15%	22,000	
Food & Beverage	24%	36,599	
Neighborhood Services	13%	20,093	
Traditional Neighborhood Conveniences	38%	58,000	
Cultural	10%	14,918	
TOTAL	100%	151,610	

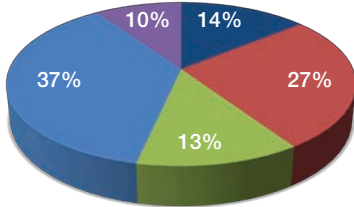


Figure 5.13I - Lakeview Village - overall tenant mix

Lakeview Village Retail Program - Lakeview Square Tenant Mix

Lakeview Square			
Unit	Size (SF)	Category	Use
A1	14,918	Civic/Cultural	Market Building/ Interactive Museum
Total Sq. Ft. Building A		14,918	
B1	6,200	Retail	Bicycle Outfitter
B2	2,100	Neighbourhood Services	Lakeview Info Centre
B3	6,000	Retail	Sport Outfitter
B4	3,000	Food & Beverage	Bakery
Total Sq. Ft. Building B		17,300	
C1	2,023	Food & Beverage	Rental Kiosk/Café
Total Sq. Ft. Building C		2,023	
D1	3,100	Food & Beverage	Aviation Diner
D2	1,800	Retail	Specialty Children's Boutique
D3	1,600	Retail	Pet Accessories
D4	1,500	Neighbourhood Services	Salon
D5	1,600	Neighbourhood Services	Convenience Store
D6	1,300	Retail	Olive Oils & Vinegars
D7	1,800	Retail	Home Accessories & Gifts
Total Sq. Ft. Building D		12,700	
E1	7,200	Food & Beverage	Microbrewery
E2	800	Food & Beverage	Ice Cream
Total Sq. Ft. Building E		8,000	
F1	1,500	Food & Beverage	Vegan Grab & Go
F2	1,300	Retail	Active Retail (Running, Yoga)
F3	2,000	Neighbourhood Services	Specialty Fitness
F4	2,000	Neighbourhood Services	Spinning Studio/ Juice Bar
F5	900	Food & Beverage	Treat Shop (Cupcakes, Donuts, etc.)
F6	3,300	Food & Beverage	BBQ Smokehouse
Total Sq. Ft. Building F		11,000	
G1	2,413	Food & Beverage	Italian Trattoria
G2	2,413	Food & Beverage	Lake-to-Table Bistro
Total Sq. Ft. Building G		4,826	
H1	10,893	Neighbourhood Services	Co-Working/ Fitness
H2	1,200	Food & Beverage	Sake/ Noodle Bar
H3	1,600	Food & Beverage	Speakeasy/Cocktail Bar
Total Sq. Ft. Building G		13,693	
TOTAL Lakeview Square (Sq. Ft.):		84,460	
TOTAL Retail/F&B/Services/Office (Sq. Ft.):		56,549	

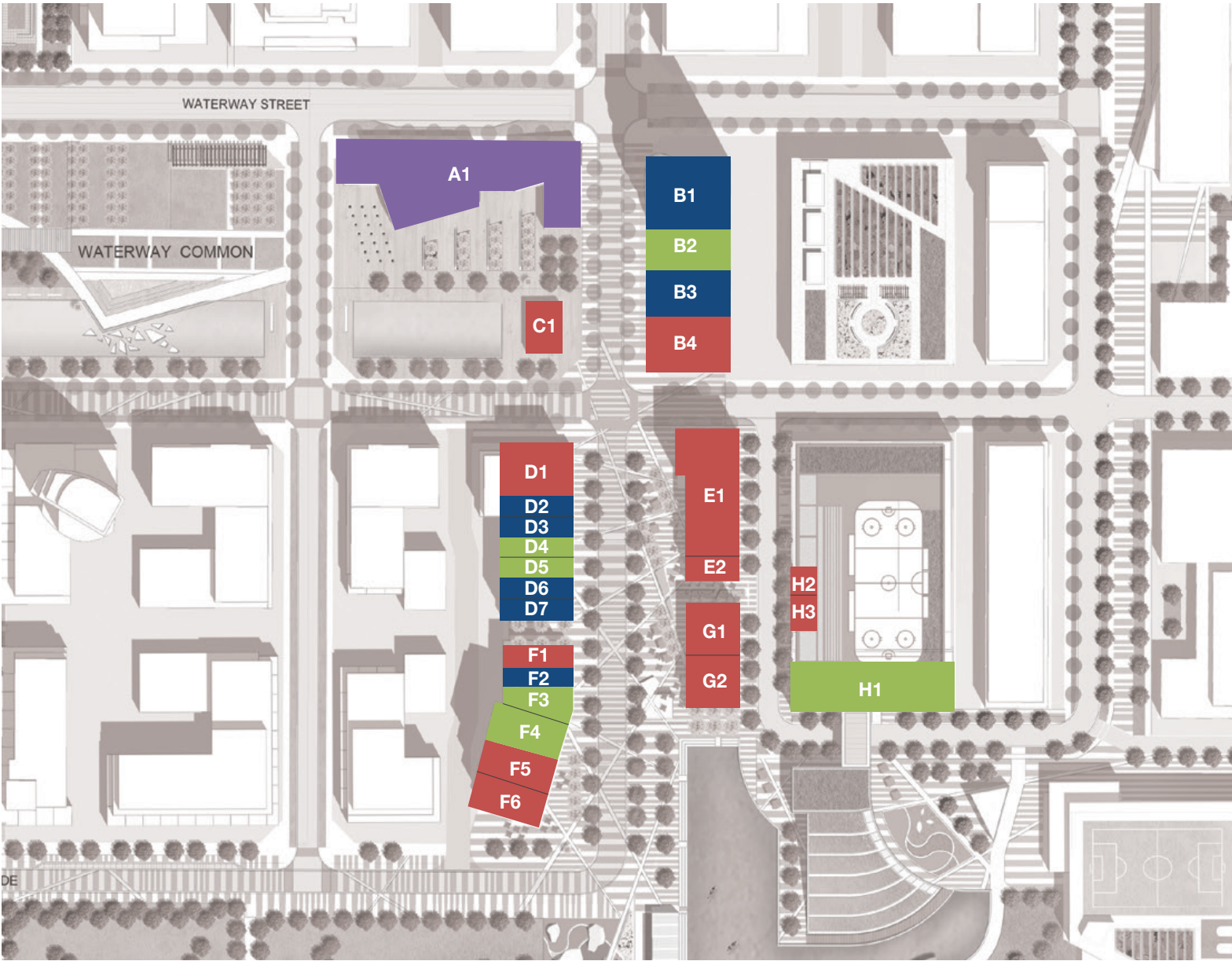


Figure 5.13m - Tenant mix - Lakeview Square

IMPLEMENTATION





Figure 6a - Aerial view of preliminary site construction in 2018

Implementation

6.1 PHASING

The LCPL lands will be phased in recognition of the following:

- Place-making in an area familiar to City residents by developing adjacent existing road and open space networks for a logical continuity of these features;
- Coordination that may be required for infrastructure placement as well as for site remediation;
- Introducing publicly accessible open spaces to produce critical mass to create attention to the future community. Initial thoughts are to have access to the Western Pier available to the public as part of work with the City of Mississauga as well as developing the first linear park in the community (Waterway Common);
- Later phases will develop the public lands with appropriate consideration of final site and building programming acceptable to the City.

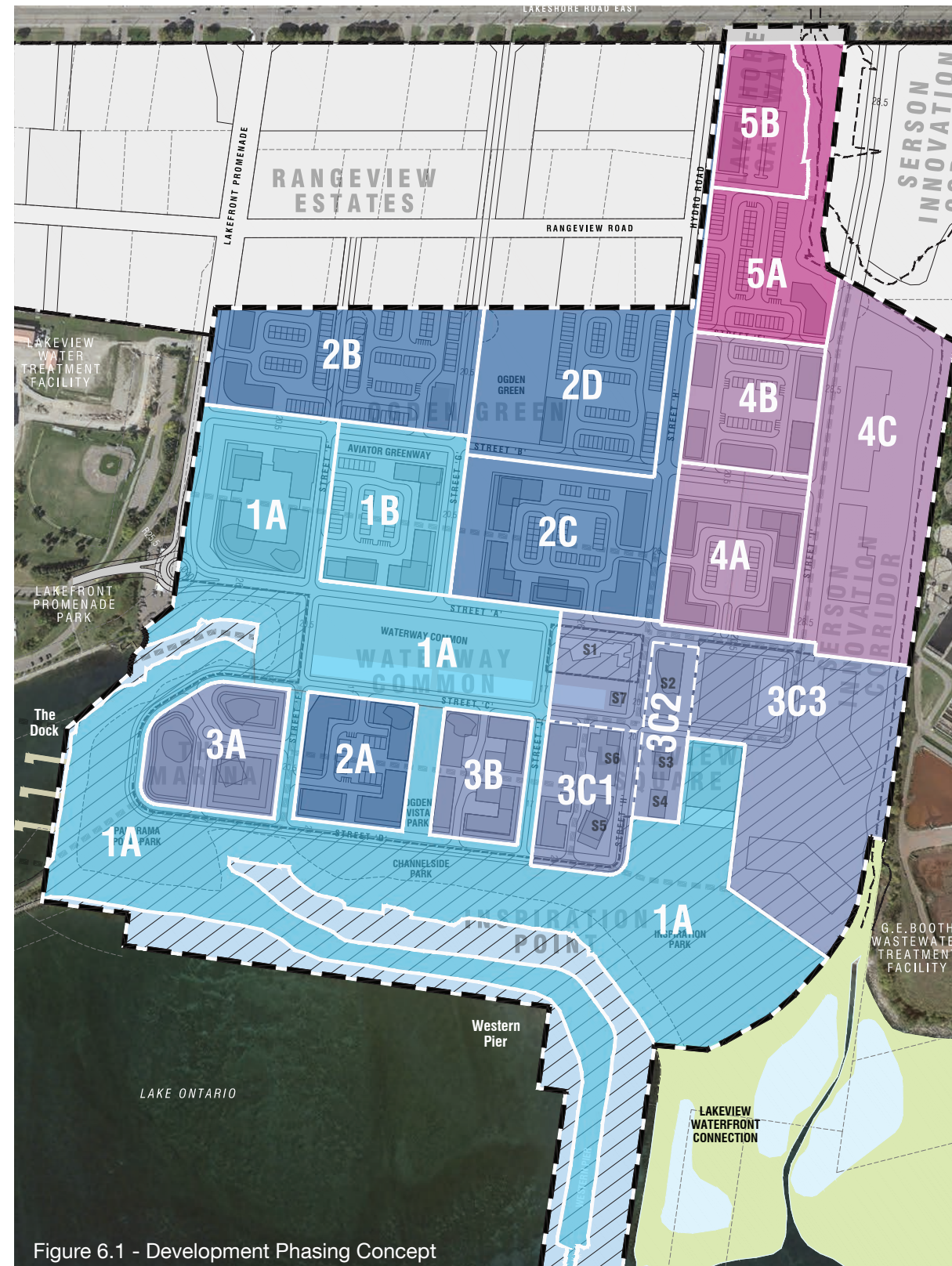


Figure 6.1 - Development Phasing Concept

6.2 SERVICING / INFRASTRUCTURE STRATEGY

6.2.1 RECOMMENDED WASTEWATER / WATER SERVICING STRATEGY

The Lakeview Village lands are located on the shores of Lake Ontario, nestled between the Lakeview Water Treatment Facility (WTF) and the G.E. Booth Wastewater Treatment Facility (WWTF). Through previous studies and recent confirmation from the Region of Peel, the adjacent water and wastewater treatment facilities possess sufficient capacity to accommodate the development of the Lakeview Village lands. The Region has suggested the methods by which the area can connect to these facilities. For wastewater servicing, this will include the installation of a pump station and force main to convey wastewater flows from the lower parts of the site to the existing trunk infrastructure on Lakeshore Road (see Figure 6.2.2g for details). For water servicing, a new looped watermain will similarly connect the new community to existing trunk infrastructure as shown in Figure 6.2.2d. The Region’s recommendations will be analysed and refined in a Functional Servicing Report.

6.2.2 STORMWATER MANAGEMENT / STORM SERVICING

The primary objectives of the stormwater management strategy for Lakeview Village include safe conveyance of runoff, quality control, and the application of low impact development (LID) measures integrated into the community in keeping with the vision of a blue and green network. Each development block will provide on-site controls to attenuate flows and provide water quality treatment through a combination of conventional and LID approaches. Runoff throughout the community will be conveyed by a network of storm sewers, road networks, and LID features to 3 outlets, in keeping with prevailing drainage patterns. LID features will also encourage reuse to address the 3mm water balance objective; infiltration measures are not proposed due to subsurface soil and water table conditions. Oil-grit separators will be implemented where required to supplement the water quality treatment approach. Figure 6.2.2e illustrates the preliminary site grading and Figure 6.2.2f highlights the corresponding storm sewer drainage design.



Figure 6.2.2a - Potential LID's within roadways

Two major water features are proposed within the community, including an urban feature along Waterway Common intended for active recreational use. The potential to reuse treated storm runoff to supply this water feature, as well as the reuse of existing subsurface infrastructure as part of the treatment mechanism, will be explored as part of the Functional Servicing and sustainability investigations. The second water feature is an open water body at the southeast corner of Lakeview Village, which is intended for paddle-boating and similar recreational uses, while also providing additional polishing of runoff prior to discharge to Lake Ontario.

Key principles of sustainable stormwater management to be further explored for the community include:

- Use of existing structures / remnant components of the former OPG power plant for stormwater management;
- Use of treated stormwater for irrigation of landscaped areas and urban farm / community garden areas;
- Use of stormwater for cooling;

- Use of treated stormwater for recreational areas (splash pads, fountains, etc.);
- Use of stormwater for car washes / non-potable water for condominium maintenance / cleaning / toilet flushing;
- Use of stormwater for maintenance (water trucks, irrigation of ROW vegetation, street sweeping and dust control during construction);
- Use of stormwater to feed end-of-pipe polishing features such as a recreational lake for canoeing, ice skating, etc.;
- Use of stormwater effluent at outlets to encourage circulation in the lake inlets / outlets;
- Integration of stormwater management measures such as LIDs and polishing wetlands as components of the landscaping / amenity areas; and
- Introducing educational signage about stormwater management goals, practices, and benefits.

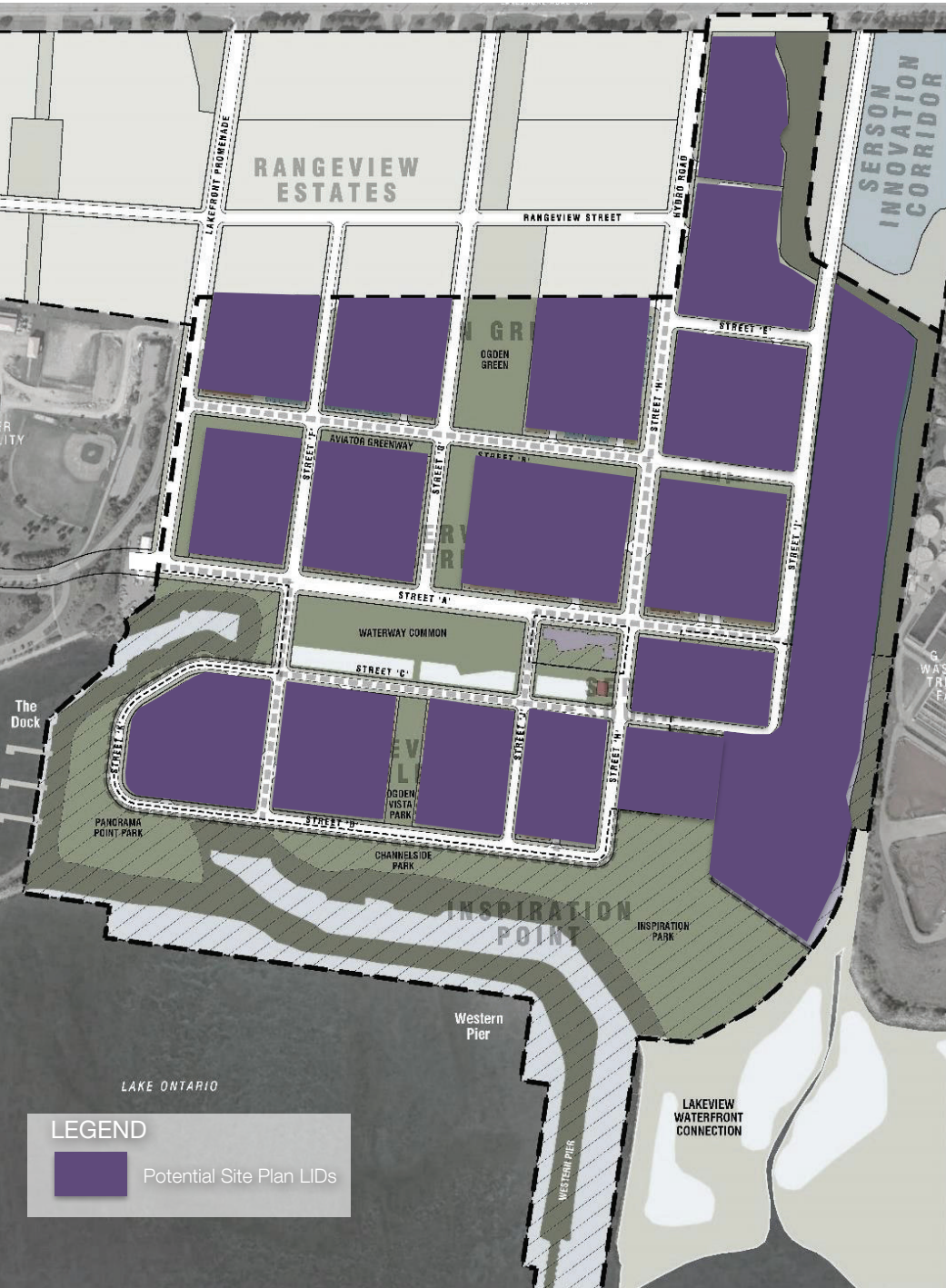


Figure 6.2.2b - Potential LID's within development parcels

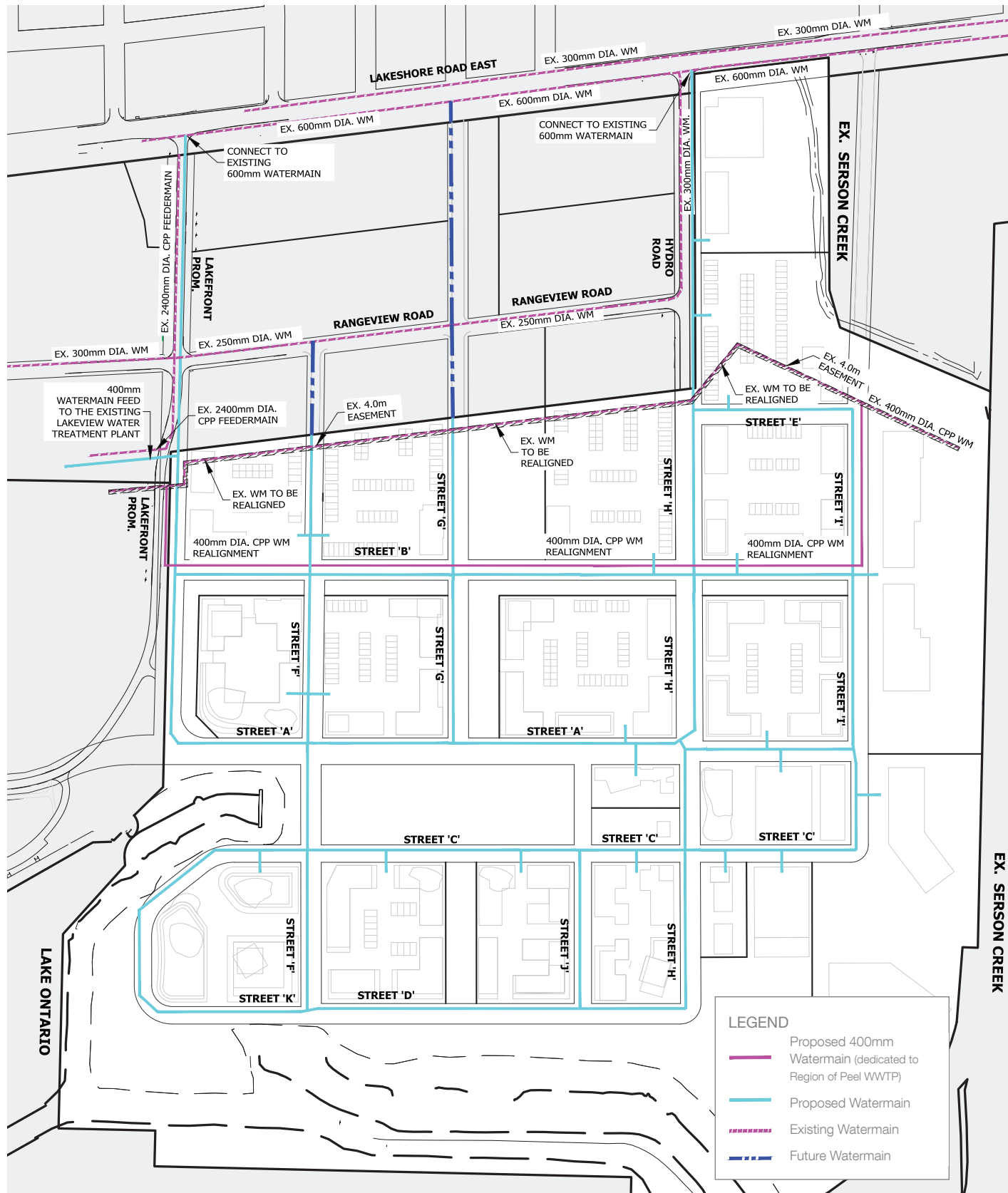


Figure 6.2.2c - Preliminary Lakeview Village Watermain Plan

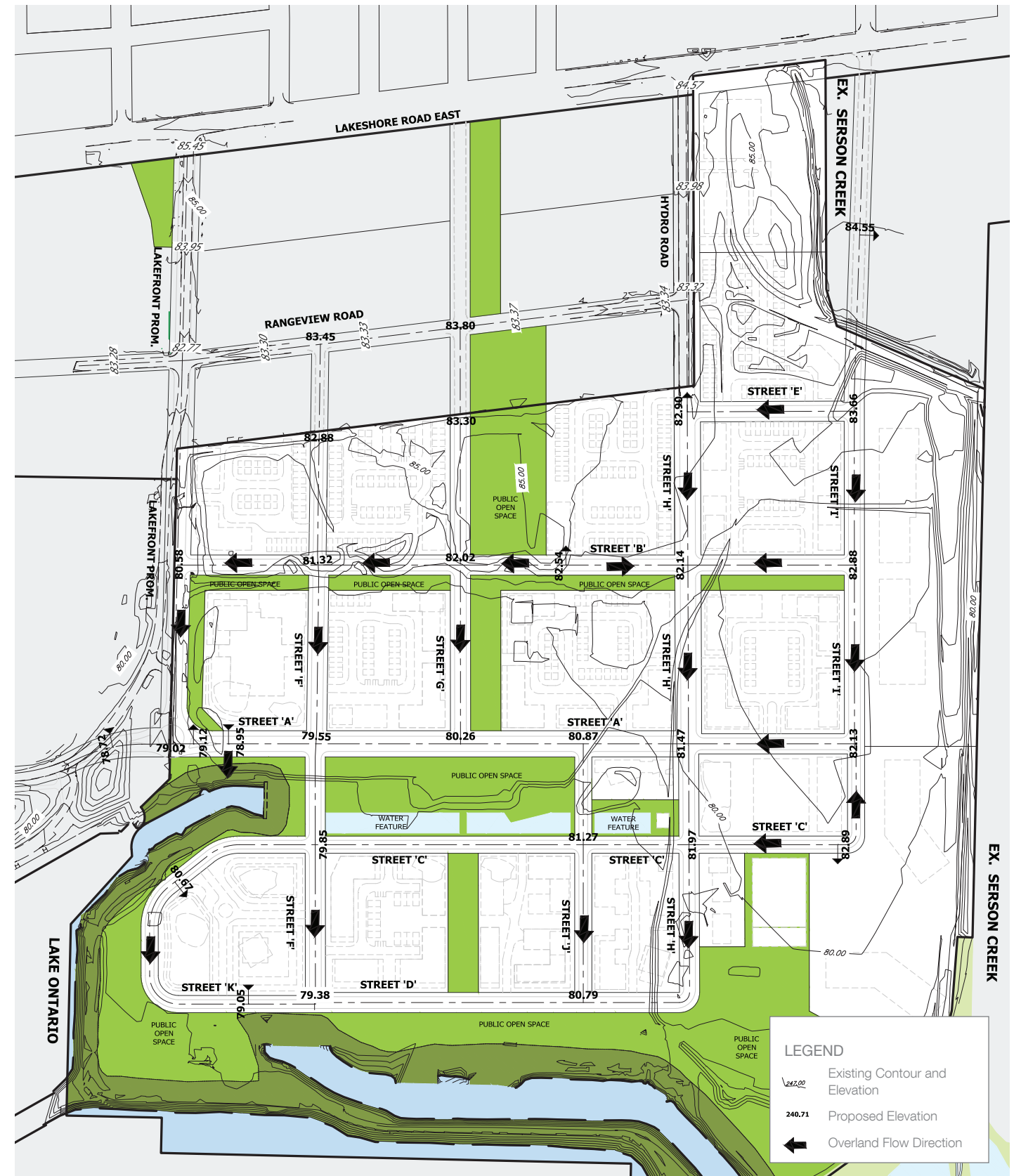
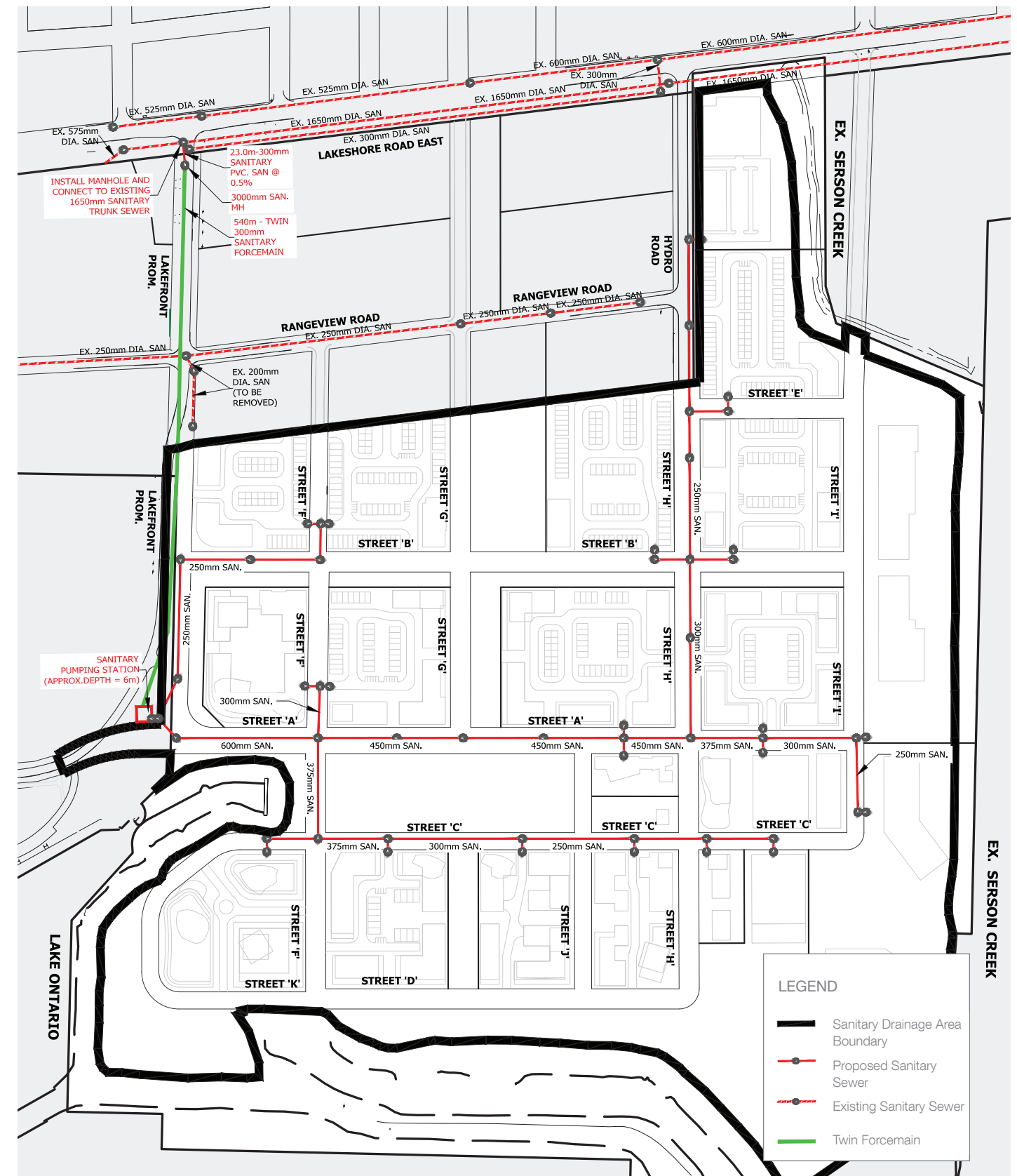
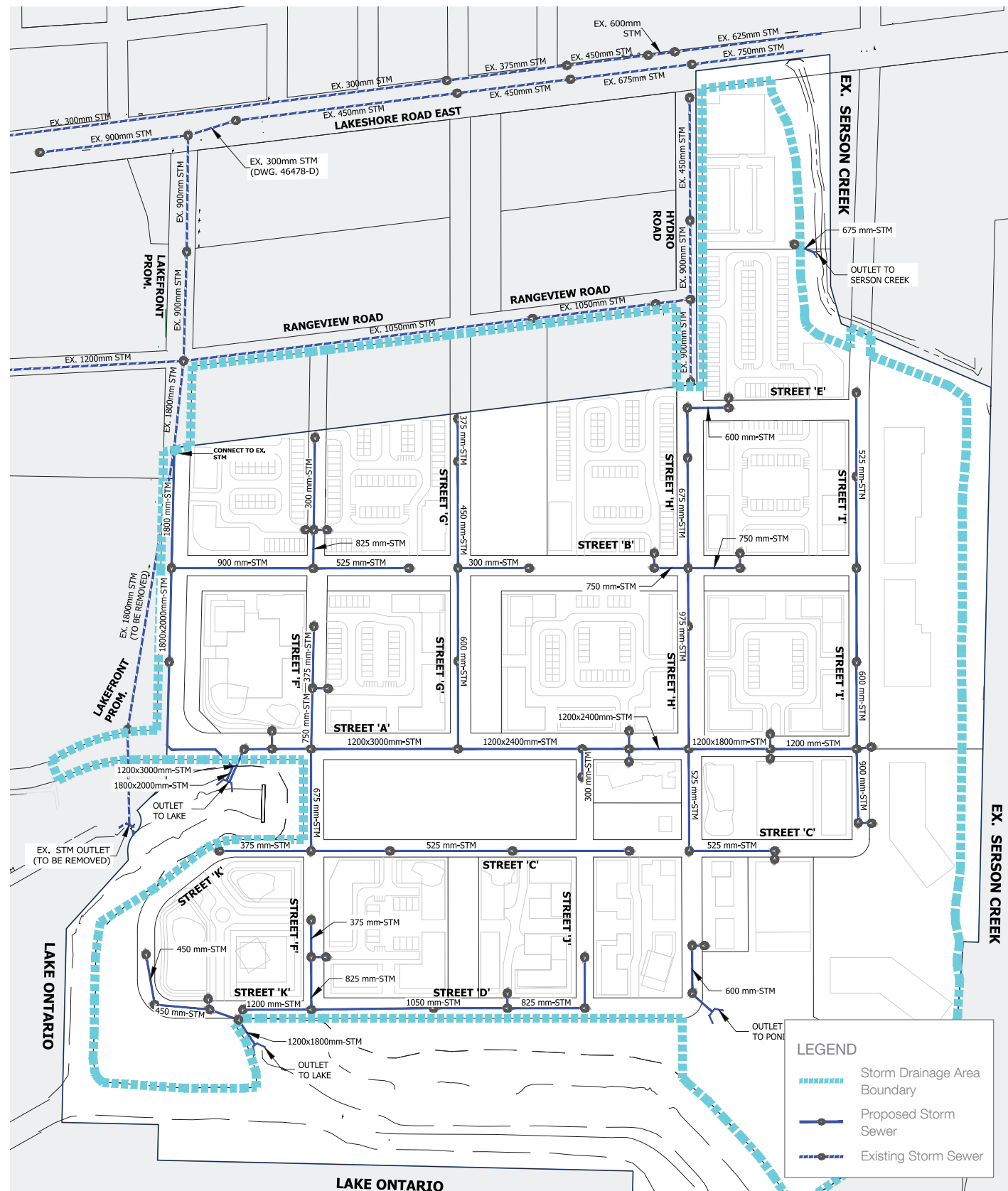


Figure 6.2.2d - Preliminary Lakeview Village Grading Plan



6.3 PARKING STRATEGY

Parking will be provided as follows:

- 1. On-street parking will be provided as appropriate and integrated into the streetscape design, balancing the needs of all modes of transportation and the public realm that share the right-of-way;
- 2. Underground parking will be encouraged on all individual development sites; however, a limited amount of surface parking may be considered on a site by site basis;
- 3. Underground and / or integrated above grade structured parking will be required for residential medium / high density and mixed-use developments;
- 4. Surface parking may be considered for:
 - Townhouse dwellings;
 - Low-rise apartment dwellings;
 - Cultural, recreational, and institutional uses; and
 - Innovation Corridor Precinct.
- 5. Freestanding and above grade structured parking near the cultural hub will incorporate elevated design elements (e.g. façade wraps, integrated into buildings). Structures will be encouraged to incorporate active uses at ground level in order to reduce negative impacts on the public realm.

Obtaining zoning by-law permissions to permit reduced parking rates and / or adopt maximum parking standards should and will be considered throughout the development, in conjunction with the provision of mixed-use developments, enhanced transit and active transportation facilities. The extent of the parking reductions shall be considered through specific zoning applications and site-specific parking demand studies.



Figure 6.3a - On-street parking integrated into streetscape design

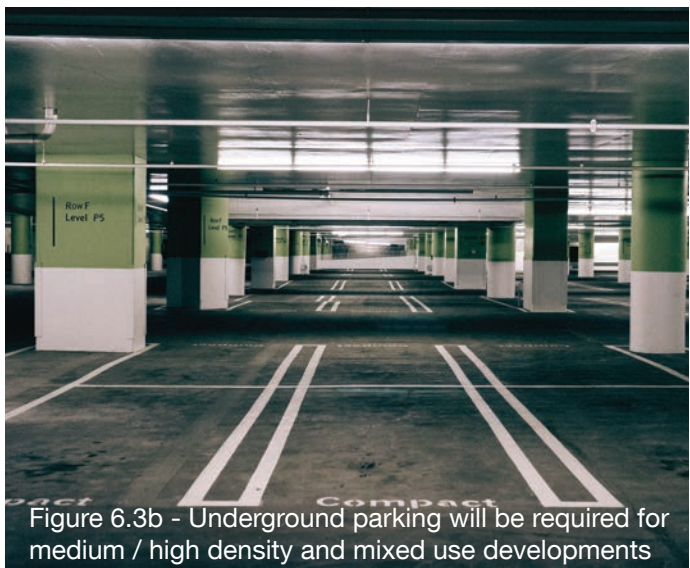


Figure 6.3b - Underground parking will be required for medium / high density and mixed use developments



Figure 6.3c - Preliminary Structured Parking Study

6.4 TRAVEL DEMAND MANAGEMENT (TDM)

Increasing vehicular traffic and congestion is a broad trend being experienced across Mississauga and the Greater Golden Horseshoe as intensification occurs. As Mississauga and surrounding municipalities mature, they experience increases in population and employment, but the opportunity to improve roadway corridors or adding roads becomes less feasible and desirable.

The Lakeview Village road network is constrained by the location of the Lakeview Wastewater Treatment Plant to the east and the lack of parallel crossings to the west. Future growth from surrounding areas will result in an increase in travel demand and congestion levels on the existing road network. The most noticeable congestion will continue to be eastbound in the morning rush hour and westbound during the afternoon rush hour along portions of Lakeshore Road. In the absence of needed transit and active transpiration infrastructure, and without appropriate travel demand strategies (beyond reduced parking provisions), development of Lakeview Village will further increase vehicular congestion levels along the corridor.

The Development of Lakeview Village as a complete neighbourhood shall promote Active Transportation and higher Transit use. Further, the proximity of the Port Credit & Long Branch GO Stations, future Light Rail Transit on Hurontario Street, and the planned rapid transit service (starting with BRT, but potentially eventually LRT) on Lakeshore Road, will promote alternatives to the private auto both for the Site and the surrounding area, which will serve to reduce the vehicular congestion impacts noted above. Substantial benefit to the existing community will also be provided by virtue of the planned Transit and Active Transportation infrastructure.

Development applications will be accompanied by focused and site-specific transportation, parking and traffic studies. These studies will address, among other things, site-specific strategies for limiting impacts on the transportation network, where appropriate, including measures such as:

- Reduced parking standards and shared parking strategies;
- Transportation demand management;
- Transit oriented development;
- Pedestrian / cycling connections; and
- Access management plans.

While it will not be possible to avoid future increases in vehicular congestion, key mitigation strategies will mitigate the impacts to the transportation network, including:

- Transportation Demand Management (TDM) Measures such as:
 - Capping the supply of residential and employee parking spaces;
 - Transit incentive programs (e.g. transit fare card provided by developer to residents; buildings include real-time transit schedule information display);
 - Creation of compact, walkable, mixed-use development centered around high-quality transit and active transportation;
 - Enhanced pedestrian and cycling connections and facilities (including enhanced connections to, and improved facilities along Lakeshore Road);
 - Programs (e.g. joining a local Smart Commute transportation management association, Car Share, etc.);
 - Limiting access to sites near intersections; and
 - Intersection improvements – operational and / or physical.
- The City will encourage Transportation Demand Management measures, where appropriate, in the Lakeshore Corridor and as a part of any significant redevelopment projects outside of the corridor.



Figure 6.4a - Enhanced cycling connections



Figure 6.4b - Intersection improvements as a transportation demand management measure

6.5 TRANSPORTATION INFRASTRUCTURE REQUIREMENTS

Phasing Principles

Phasing of development within the study area is currently being finalized and is being coordinated with the overall Development Program and Servicing / Infrastructure Strategy. The following principles will guide the phasing of development within Lakeview Village:

- Infrastructure and development shall be phased to ensure that growth occurs in a logical and fiscally sustainable manner.
- Development will occur in a manner that does not place unnecessary costs on new or existing residents and/or the municipality.
- Development should occur in tandem with the provision of appropriate levels of infrastructure.

These first three principles emphasize maximizing the use of existing infrastructure. Where possible, new development should make use of the existing roadways and other infrastructure, such as Lakefront Promenade and Hydro Road. Initially, this would include areas with access from the City road network and in close proximity to the existing sanitary sewers and watermain in the study area. As development proceeds, subsequent phases should extend logically from the streets installed in the prior phase.

Large infrastructure projects, such as the north-south New Ogden Avenue connection to Lakeshore Road should be deferred to the latter phases of development, if feasible. Similarly, construction of the New Haig Road connection should coincide with development of the Serson Innovation Centre and Campus.

Road phasing is adaptive to evolving matters such as infrastructure timing and other inputs. Given that it will be at least 10 years from the completion of this Development Master Plan and further development of the Lakeview Village area, the road network phasing plan is flexible to allow development to proceed in response to evolving transportation demands, servicing infrastructure timing, and other inputs.

Road Network Phasing

Development of the Lakeview Village, and related adjacent roads, is expected to be phased in general accordance to the following primary nodes (by District):

1. West portion of Waterway Common, The Marina, and Inspiration Point.
2. West portion of Ogden Green.
3. Lakeview Square and south portion of Serson Innovation Corridor.
4. East portion of Ogden Green and north portion of Serson Innovation Corridor.
5. Lakeshore Gateway.

The improvements to the City road network required by 2031 full build-out are listed below. The improvements to the road networks are recommended to alleviate traffic congestion and capacity issues along the Lakeshore Road corridor and intersections. The initial assessment of required infrastructure to be confirmed through future transportation analyses and confirmation of the broader development phasing program (which is ultimately driven by market forces).

- Lakefront Promenade and Hydro Road connections to Lakeshore Road.
- Implementation of Lakeshore Connecting Communities BRT on Lakeshore Road.
- Dedicated northbound left turn lanes at Lakeshore Road and Lakefront Promenade, New Ogden Avenue, and Hydro Road.
- New Ogden Avenue connection to Lakeshore Road by 2031 (to be deferred if feasible).
- New Haig Boulevard connection to Lakeshore Road by 2041.



Figure 6.5a - Road network improvements



Figure 6.5b - Cycle network improvements

6.6 NEXT STEPS

The full MOP framework is approved and in place setting out the formal land use policy planning vision for the Lakeview Village community. This DMP provides the details and plans for executing the vision and must be reviewed by City staff with the ultimate goal of City Council endorsement.

The next steps for implementing the Development Master Plan can be summarized as follows:

- Completion of area-wide studies to support the Plan and in fulfillment of any MOP requirements if required;
- Consult with the City to prepare detailed development applications and submit them accordingly;
- Continue with community engagement and liaising with other area landowners appropriate for the stages of the ongoing advancement of site development;
- City Council endorsement of this DMP anticipated for January 2019;
- Continue discussions with the City and other approval authorities with regards to other aspirational area requirements which directly or indirectly impact the Plan including but not limited to cost-sharing discussions, review of cultural facility planning, and waterfront park planning and programming.

